














NOTES:

- PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
- PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS, FIRE RATED & STC RATED TO BCA REQUIREMENTS.
- BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
- FOR ALL BRACING, TIE-DOWN, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
- REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
- REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
- REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
- REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
- ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3740 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
- PROVIDE 50mm SETDOWN TO FLOORS IN ALL WET AREAS.
- ALL FLOOR WASTES TO BE CONCEALED TYPE WITH IN-FILL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN)
- ALL DOWNPIPES TO BE Ø100 COLORBOND U.N.O.
- DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB-CONSULTANTS DOCUMENTS.
- THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFICATES & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR FAILURE TO DO SO.
- ALL WALLS & CEILINGS TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
- ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
- PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
- ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS)
- CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
- ALL DRAWINGS TO BE READ IN CONJUNCTION WITH DESIGN VIBE P/L SPECIFICATION DOCUMENT.
- ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1657
- ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1891.1 & AS 4882.

LEGEND:

AC - AIR CONDITIONING	OG - OPALQUE GLASS
AL - ALUMINIUM	OHC - OVERHEAD CUPBOARD
AP - ACCESS PANEL	PB - PLASTERBOARD LINING
APP - APPLIANCE CUPBOARD	PD - PLUMBING DUCT
AS - AUSTRALIAN STANDARDS	PDR - POWDER ROOM
AVN - AWNING	PL - PLANTER
BCA - BUILDING CODE OF AUSTRALIA	PTY - PANTRY CUPBOARD
BO - BALCONY OUTLET	PV - PERMANENT VENT
BR - BROOM CUPBOARD	REF - REFRIGERATOR
BS - BAR SINK	RH - RANGEHOOD
BT - BUCKET TRAP	RL - REFERENCE LEVEL
CAV - CAVITY	RWH - RAINWATER HEAD
CBD - CUPBOARD	S - SMOKE DETECTOR
CCNC - CONCRETE	SD - SLIDING DOOR
CCS - CAVITY SLIDING DOOR	SEL - SELECTED
CTS - CENTRES	SH - SHELVES
DN - DOWN	SHR - SHOWER RECESS
DRY - CLOTHES DRYER	SK - SINK
DP - DOWNPIPE	SO - SOAP DISPENSER
DW - DISHWASHER	SP - SPITTER
E - EMERGENCY LIGHTING	SPR - RAINWATER SPREADER
FC - FIBRE CEMENT SHEETING	SS - STAINLESS STEEL
FD - FIRE RATED DOOR	SSL - STRUCTURAL SURFACE LEVEL
FE - FIRE EXTINGUISHER	SWO - STORMWATER OUTLET
FLL - FINISHED FLOOR LEVEL	SWRM - STORMWATER RISING MAIN
FG - FIXED GLASS	T - LAUNDRY TUB
FH - FIRE HYDRANT	TR - TOWEL RAIL
FHR - FIRE HOSE REEL (38m)	TRL - TOILET ROLL HOLDER
FSL - FINISHED SURFACE LEVEL	TS - THERMAL SMOKE ALARM
FW - FLOOR WASTE	TYP - TYPICAL
GB - GLASS BRICKS	UB - UNDER BENCH OVEN
GP - GULLY PIT	UR - URINAL
GW - GLASS WASHER	US - UNDERSIDE
HB - HAND BASIN	VB - VANITY BASIN
HC - HOSECOCK	VM - VACUUM/AID
HD - HAND DRYER	W - SLIDING WINDOW
HP - HOTPLATES / COOKTOP	WC - WATER CLOSET
HWS - HOT WATER SERVICE	WM - WASHING MACHINE
K & C - KERB & CHANNEL	WO - WALL OVEN
LC - LAUNDRY CHUTE	WPM - WATER PROOF MEMBRANE
LIN - LINEN CUPBOARD	
LV - LOUIRE	
M - MIRROR	
MTSD - MULTI-TRACK SLIDING DOOR	
MV - MECH. VENTILATION DUCT	
MW - MICROWAVE OVEN	

TYPICAL WALL TYPES:

 90mm TIMBER / STEEL STUD WALL  LAMINATED TOILET PARTITION  190mm CONCRETE BLOCKWORK - 110mm (RECYCLED) MASONRY BRICK SURROUND  190mm CONCRETE BLOCKWORK - 110mm (RECYCLED) MASONRY BRICK TO FACADE  250mm DOUBLE MASONRY BRICK WALL - 2 x 110mm MASONRY BRICK WALLS WITH 30mm CAVITY  190mm CONCRETE BLOCKWORK  150mm REINFORCED CONCRETE TILT-UP WALL PANEL / COLUMN	 175mm REINFORCED CONCRETE TILT-UP WALL PANEL / COLUMN  250mm REINFORCED CONCRETE COLUMN / SPRAYED CONCRETE RETAINING WALL  300mm REINFORCED CONCRETE COLUMN / SPRAYED CONCRETE RETAINING WALL  350mm REINFORCED CONCRETE COLUMN / SPRAYED CONCRETE RETAINING WALL  190mm CONCRETE BLOCKWORK (PLANTED GREEN WALL) TO MANUFACTURERS DETAIL  SITE MEASURED CURTAIN WALL
---	--

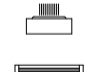






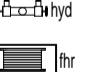
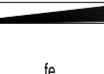

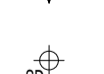


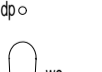




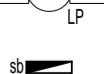
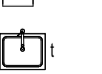

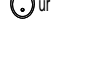




ENERGY PROVISIONS

- EXTERNAL WALLS NEED AN OVERALL RATING OF R2.8
- EXTERNAL ROOF / CEILING NEEDS AN OVERALL RATING OF R3.2
- A MONITORING SYSTEM FOR CARPARKING A SUSPENDED CEILING ACHIEVES R1.50 INSULATION BUT LEANS TO WILL NEED TO BE ADDED (POSSIBLY PLANKS UNDER THE REFLECTIVE SARKING)
- EXTERNAL CONCRETE WALLS WITH CAVITY AND PLASTERBOARD ACHIEVE R0.40 SO ADEQUATE WALL INSULATION IS NEEDED. (NOTE: CAVITY BATTEN DEPTH MAY NEED TO BE INCREASED SO AS NOT TO COMPRESS INSULATION)
- EXTERNAL COLUMN WEIGHTS SHOULD ACHIEVE R0.4 SO WALL INSULATION OF R2.5 WILL BE NEEDED FOR THESE
- THESE DRAWINGS ARE TO BE READ IN ACCORDANCE WITH McUTOCHON MACDONALD ENERGY EFFICIENCY REPORT

**PRELIMINARY
NOT FOR CONSTRUCTION**

PART OF BUILDING	CLASS	APPROX FLOOR AREA
RETAIL / SALES	6	6,102 m ²
CARPARKING	7a	6,483 m ²

SERVICES LEGEND

 AC SPLIT SYSTEM CONDENSER	 PS STORMWATER PUMP STATION CONTROL PANEL
 AC SPLIT SYSTEM HEAD	 HWS HOT WATER SERVICE (SOLAR GAS OR HEATPUMP TO COMPLY WITH ENERGY EFFICIENCY CODE)
 AC CONTROL PANEL	 SW SWIMMING POOL FILTER & PUMP
 STEEL BOLLARD	 VAC VACUUM/AID
 SOLAR PANEL	 EL EMERGENCY LIGHTING
 FIRE CONTROL PANEL	 EHL DUAL OUTLET PILLAR HYDRANT
 ELECTRICAL METER BOX / DISTRIBUTION BOARD	 FR FIRE HOSE REEL
 2A 40E DRY CHEMICAL FIRE EXTINGUISHERS	 S STORMWATER OUTLET
 SMOKE DETECTOR	 GP GULLY PIT
 SMOKE ALARMS TO COMPLY WITH B.C.A CLASS 1 & 10 AHT 3.2.2 & WITH A.S.3786	 BT BUCKET TRAP (WITH REMOVABLE BASKET)
 SQUARE HOLLOW SECTION POST	 DP DOWNPIPE
 HARDWOOD TIMBER POST	 WC WATER CLOSET
 EXIT SIGN	 HB HAND BASIN
EXIT ARROW	UB UNDER BENCH FRIDGE
EXTERIOR LIGHTPOLE	VB VANITY BASIN
SUB BOARD	TUB
MECH VENTILATION	UR URINAL

CONSTRUCTION TO COMPLY WITH:

BCA CLASS 1 & 10 & CLASS 2-9	AS 3700
MASONRY	AS 3600
CONCRETE CONSTRUCTION	AS 1250 / AS 1538 / AS 4100
STEEL CONSTRUCTION	AS 2327
COMPOSITE STEEL & CONCRETE	AS 1564
ALUMINIUM CONSTRUCTION	AS 1694
TIMBER CONSTRUCTION	AS 3740
WET AREAS	AS 1288
GLASS INSTALLATION	AS 1694 / AS 2057
PROTECTION FROM TERMITES	AS 2179.1
ROOF CONSTRUCTION-GUTTER/DRAIN FLASHING	AS 2049 / AS 2050
-CONCRETE TILES	AS 1562.1
-METAL ROOFING	AS 3796
SMOKE ALARMS	AS 1428.1
DISABLED AMENITIES / ACCESS MOBILITY	AS 3500
PLUMBING & DRAINAGE	AS 1521.1
EMERGENCY LIGHTING	AS / NZS 2293
FIRE HYDRANTS	AS 2419
FIRE HOSE REELS	AS 2444
FIRE EXTINGUISHERS	AS / NZS 3013
ELECTRICAL INSTALLATIONS	AS / NZS 2904
DAMP PROOF COURSES & FLASHING	AS / NZS 1688
MECHANICAL VENTILATION & A/C	AS 2047
WINDOWS	AS 2870
RESIDENTIAL SLABS & FOOTINGS	AS 3740
WATERPROOFING OF WET AREAS	AS 1428
DISABLED ACCESS CODE	

LANDSCAPING

LANDSCAPING AREA	AREA
LANDSCAPING AREA 1	185.8 m ²
LANDSCAPING AREA 2	20.4 m ²
LANDSCAPING AREA 3	58.7 m ²
LANDSCAPING AREA 4	14.5 m ²
LANDSCAPING AREA 5	442.2 m ²
LANDSCAPING AREA 6	21.0 m ²
LANDSCAPING AREA 7	9.7 m ²
LANDSCAPING AREA 8	33.1 m ²
	785.6 m ²

DRAWING SCHEDULE

DWG No.	DRAWING TITLE
DA-1.00	TITLE SHEET (STAGE 1)
DA-1.01	LOCALITY PLAN (STAGE 1)
DA-1.02	SITE PLAN (STAGE 1)
DA-1.03	STAGING, EXTENT OF EXISTING & LOCALITY PLAN (STAGE 1)
DA-1.04	LOWER BASEMENT FLOOR PLAN (STAGE 1)
DA-1.05	GROUND FLOOR PLAN (STAGE 1)
DA-1.06	LEVEL ONE FLOOR PLAN (STAGE 1)
DA-1.07	ROOF PLAN (STAGE 1)
DA-2.01	ELEVATION SHEET 1 (STAGE 1)
DA-2.02	ELEVATION SHEET 2 (STAGE 1)
DA-2.03	ELEVATION SHEET 3 (STAGE 1)
DA-3.01	SECTIONS (STAGE 1)

MIN. FIRE RESISTANCE LEVEL (FRL) REQUIREMENTS: CLASS 6

BUILDING ELEMENT	MIN. FIRE
BUILDING EXTERNAL WALLS (LOADBEARING) < 1.5m OF ADJACENT PROPERTIES	- FRL 180/180/180
BUILDING EXTERNAL WALLS (LOADBEARING) 1.5m to 3m OF ADJACENT PROPERTIES	- FRL 180/120/90
BUILDING EXTERNAL WALLS (LOADBEARING) 3m to 9m OF ADJACENT PROPERTIES	- FRL 120/30/0
BUILDING EXTERNAL WALLS (LOADBEARING) 9m to 18m OF ADJACENT PROPERTIES	- FRL 180/60/-
BUILDING EXTERNAL WALLS (LOADBEARING) > 18m OF ADJACENT PROPERTIES	- FRL -/-/-
BUILDING EXTERNAL WALLS (NON - LOADBEARING) < 1.5m OF ADJACENT PROPERTIES	- FRL -/180/180
BUILDING EXTERNAL WALLS (NON - LOADBEARING) 1.5m to 3m OF ADJACENT PROPERTIES	- FRL -/120/90
BUILDING EXTERNAL WALLS (NON - LOADBEARING) > 3m OF ADJACENT PROPERTIES	- FRL -/-/-
EXTERNAL COLUMN < 3m OF ADJACENT PROPERTIES	- FRL 180/-/-
EXTERNAL COLUMN > 3m OF ADJACENT PROPERTIES	- FRL -/-/-
COMMON WALLS & FIRE WALLS	- FRL 120/120/120
BUILDING INTERNAL WALLS - FIRE-RESISTING LIFT & STAIR SHAFTS (LOADBEARING)	- FRL 180/120/120
BUILDING INTERNAL WALLS - FIRE-RESISTING STAIR SHAFTS (NON - LOADBEARING)	- FRL -/120/120
BUILDING INTERNAL WALLS BOUNDING PUBLIC CORRIDORS, LOBBIES ETC. (LOADBEARING)	- FRL 180/-/-
BUILDING INTERNAL WALLS BOUNDING PUBLIC CORRIDORS, LOBBIES ETC. (NON - LOADBEARING)	- FRL -/-/-
BUILDING INTERNAL WALLS BETWEEN OR BOUNDING (LOADBEARING)	- FRL 180/-/-
BUILDING INTERNAL WALLS BETWEEN OR BOUNDING (NON - LOADBEARING)	- FRL -/-/-
ALL OTHER LOADBEARING INTERNAL WALLS & COLUMNS	- FRL 120/-/-
ROOF CONSTRUCTION / COVERING	- NON COMBUSTIBLE

MIN. FIRE RESISTANCE LEVEL (FRL) REQUIREMENTS: CLASS 7a

BUILDING ELEMENT	MIN. FIRE
BUILDING EXTERNAL WALLS (LOADBEARING) < 1.5m OF ADJACENT PROPERTIES	- FRL 120/120/120
BUILDING EXTERNAL WALLS (LOADBEARING) 1.5m to 3m OF ADJACENT PROPERTIES	- FRL 120/90/60
BUILDING EXTERNAL WALLS (LOADBEARING) 3m to 9m OF ADJACENT PROPERTIES	- FRL 120/30/-
BUILDING EXTERNAL WALLS (LOADBEARING) 9m to 18m OF ADJACENT PROPERTIES	- FRL -/-/-
BUILDING EXTERNAL WALLS (LOADBEARING) > 18m OF ADJACENT PROPERTIES	- FRL -/120/120
BUILDING EXTERNAL WALLS (NON - LOADBEARING) < 1.5m OF ADJACENT PROPERTIES	- FRL -/90/60
BUILDING EXTERNAL WALLS (NON - LOADBEARING) 1.5m to 3m OF ADJACENT PROPERTIES	- FRL -/-/-
BUILDING EXTERNAL WALLS (NON - LOADBEARING) > 3m OF ADJACENT PROPERTIES	- FRL -/120/-/-
EXTERNAL COLUMN < 3m OF ADJACENT PROPERTIES	- FRL -/120/-/-
EXTERNAL COLUMN > 3m OF ADJACENT PROPERTIES	- FRL -/-/-
COMMON WALLS & FIRE WALLS	- FRL 120/120/120
BUILDING INTERNAL WALLS - FIRE-RESISTING LIFT & STAIR SHAFTS (LOADBEARING)	- FRL 120/120/120
BUILDING INTERNAL WALLS - FIRE-RESISTING STAIR SHAFTS (NON - LOADBEARING)	- FRL -/120/120
BUILDING INTERNAL WALLS BOUNDING PUBLIC CORRIDORS, LOBBIES ETC. (LOADBEARING)	- FRL 120/-/-
BUILDING INTERNAL WALLS BOUNDING PUBLIC CORRIDORS, LOBBIES ETC. (NON - LOADBEARING)	- FRL -/-/-
BUILDING INTERNAL WALLS BETWEEN OR BOUNDING (LOADBEARING)	- FRL 120/-/-
BUILDING INTERNAL WALLS BETWEEN OR BOUNDING (NON - LOADBEARING)	- FRL -/-/-
ALL OTHER LOADBEARING INTERNAL WALLS & COLUMNS	- FRL 120/-/-
ROOF CONSTRUCTION / COVERING	- NON COMBUSTIBLE

MIN. FIRE RESISTANCE LEVEL (FRL) REQUIREMENTS: CARPARKS

BUILDING ELEMENT	MIN. FIRE
EXTERNAL WALLS (LOADBEARING) < 3m	- FRL 60/60/60
EXTERNAL WALLS (NON - LOADBEARING) < 3m	- FRL -/60/60
INTERNAL WALLS > 3m	- FRL -/-/-
INTERNAL WALLS - (LOADBEARING) OTHER THAN ROOF SUPPORT (NOT USED FOR CARPARKING)	- FRL 60/-/-
INTERNAL WALLS - ROOF SUPPORT ONLY (NOT USED FOR CARPARKING)	- FRL -/-/-
INTERNAL WALLS - (NON - LOADBEARING)	- FRL -/-/-
FIRE WALLS - (CARPARK)	- FRL 60/60/60
COLUMNS > 3m - ROOF SUPPORT ONLY (NOT USED FOR CARPARKING)	- FRL -/-/-
COLUMNS - STEEL (OTHER THAN ONE COVERED BY ROOF SUPPORT ONLY)	- FRL 60/-/- or 28m ² / tonne
COLUMNS - ANY OTHER	- FRL 60/-/-
BEAMS < 3m - STEEL FLOOR BEAM IN CONTINUOUS CONTACT WITH A CONCRETE FLOOR SLAB	- FRL 60/-/- or 30m ² / tonne
BEAMS < 3m - ANY OTHER	- FRL 60/-/-
BEAMS > 3m	- FRL -/-/-
LIFT SHAFT	- FRL -/-/-
STAIR SHAFT - FIRE RESISTING (WITHIN CARPARK)	- FRL 60/60/60
ROOF CONSTRUCTION / COVERING	- NON COMBUSTIBLE



Attachment 2 - 16-24 Charles Street and 9 Canal Street Launceston
Plans to be endorsed (Pages = 90)



R.P.D.	
LOTS 1-3	on SP123357
LOT 1	on SP944
LOT 1	on RP246269
LOT 1	on D41792
LOT 1	on D41793
LOT 1	on RP219736

SITE AREA - 10594m²
SITE COVER - 3402.4m² (32%)
BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
CONSTRUCTION TYPE = TYPE B
CARPARKING PROVIDED = 197 + 9 DISABLED

SITE COVER	
TOTAL SITE COVER	3402.4 m ²
	3402.4 m ²

NET LETTABLE AREA			
TENANCY 0.01	BULKY GOODS SALES	HERITAGE REFURBISHMENT	184.6 m ²
TENANCY 1.01	BULKY GOODS SALES	PROPOSED	400.4 m ²
TENANCY 1.02	RESTAURANT	HERITAGE REFURBISHMENT	235.0 m ²
TENANCY 1.03	BULKY GOODS SALES	PROPOSED	1801.5 m ²
TENANCY 1.05(LOWER)	RESTAURANT	HERITAGE REFURBISHMENT	162.5 m ²
TENANCY 1.05(UPPER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.9 m ²
TENANCY 2.01	RESTAURANT	HERITAGE REFURBISHMENT	118.4 m ²
TENANCY 2.02 INSIDE	CHILDCARE	PROPOSED	868.0 m ²
TENANCY 2.02 OUTSIDE	CHILDCARE	PROPOSED	841.4 m ²

GROSS FLOOR AREA	
LOWER BASEMENT	
CARPARKING	6482.6 m ²
GOODS LIFT	12.4 m ²
LIFT	11.4 m ²
LOADING DOCK	12.9 m ²
PLANT / ELECTRICAL ROOM	60.0 m ²
PUMP / TANK ROOM	126.4 m ²
REFUSE STORE	20.7 m ²
SERVICE CORRIDOR	39.9 m ²
STAIR 3	16.4 m ²
STAIR 4	10.5 m ²
TENANCY 0.01	226.9 m ²
TENANCY 1.03 STORE	97.1 m ²
UPPER BASEMENT	
GROUND FLOOR	226.9 m ²
AMENITIES	50.2 m ²



design.vibe
Pty. Ltd.

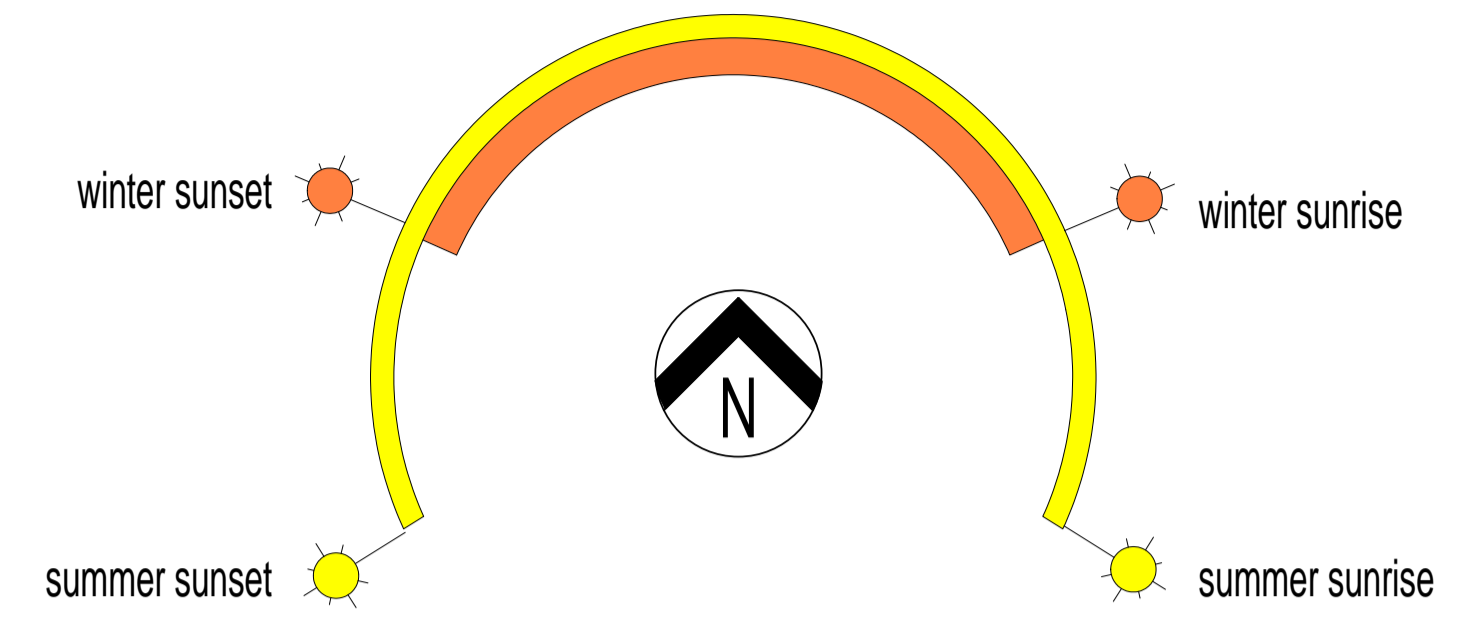
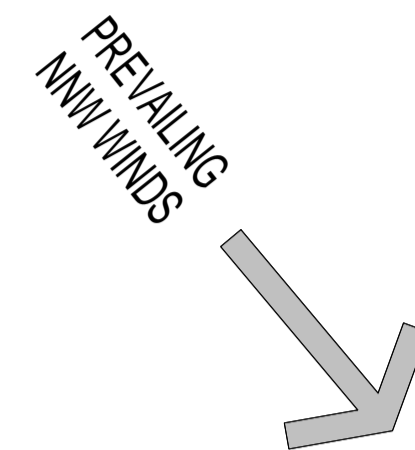
ACN 102 812 231 /
ABN 13 102 812 231
CRICOS A.C. No. 1028223
VIC. Reg. No. DP AD 36611 /
EP 122 8642
TAS. Reg. No. CC 6852 T

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 NO. 401 Luchun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

architectural building design

===== INDICATES PEDESTRIAN ACCESS THROUGH DEVELOPMENT, WITH FUTURE LANDSCAPED FEATURE LINK TO ADJACENT PARKLANDS



GENERAL NOTES:
This design & documentation remains the sole property of Copyright © design.vibe Pty. Ltd. No reproduction, use or disclosure is permitted without the written permission of this firm. All Rights Reserved.
The designer and/or project manager is not responsible for the design and associated documentation prior to the commencement of any building works, any application or discharge to a government authority and/or regulator.
Commencement of any works relating to these plans and documentation deems that the recipient of these plans is satisfied that they are correct in all respects and that they fully represent the intent of the project.
If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
design.vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless that do not comply with Local Authority / Public-Client approved documents.
All works are to comply with the following:
- The Building Code of Australia
- The Queensland Building Act
- All relevant Australian Standards
- All relevant Local Authority By-Laws and Planning Schemes.
DO NOT SCALE THIS DRAWING. IF IN DOUBT, ASK TO BE IN CONSTRUCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
BRILE PTY. LTD.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITERIE STREET
LAUNCESTON

consultants:

Title:
LOCALITY PLAN (STAGE 1)

no.	amend.	date
B	DA ISSUE	19/05/15
A	L.C.C. REQUESTED CHANGES	18/03/15

scale: 1:1000 job no: 10018
date: 09/12/15 sheet no: DA-101
drawn: 184 amendment: B
checked: SK sheet size: A1

1 LOCALITY PLAN
SCALE: 1:1000
DA-1.01

PRELIMINARY
NOT FOR CONSTRUCTION



1:000 B 5m
1:200 B 5m
1:100 B 5m

1 2 3 4

5 6 7 8 9 10 11 12 13 14

R.P.D.
 LOTS 1-3 on SP123357
 LOT 1 on SP944
 LOT 1 on RP246269
 LOT 1 on D41792
 LOT 1 on D41793
 LOT 1 on RP219736

SITE AREA - 10594m²
 SITE COVER - 3402.4m² (32%)
 BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
 CONSTRUCTION TYPE = TYPE B
 CARPARKING PROVIDED = 197 + 9 DISABLED

- NOTES:**
1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
 2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS. FIRE RATED & STRUTTED TO BCA REQUIREMENTS.
 3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
 4. FOR ALL BRACING, TIE-DOWN, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
 5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
 6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
 7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
 8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
 9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3540 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
 10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL WET AREAS.
 11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH INFILL COVER TO MATCH TILING (OR S/S GRATES WHERE SHOWN).
 12. ALL DOWNPIPES TO BE 1100 COLORBOND LIND.
 13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB-CONSULTANTS DOCUMENTS.
 14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFIER & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR FAILURE TO DO SO.
 15. ALL WALLS & CEILINGS TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
 16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
 17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
 18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
 19. CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DIVG.
 20. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE P/L SPECIFICATION DOCUMENT.
 21. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1657.
 22. ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1891.1 & AS 488.2.



design vibe Pty. Ltd.

ACN 102 812 231 /
 ABN 13 102 812 231
 CRICOS R.A.C. No. 1000273
 VIC. Reg. No. 89, AD 36611 /
 QPS 102 8642
 TAS. Reg. No. CC 6652 T

australia:
 2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Beiliuxiang
 5 NO. 401 Luchun, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This design & documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction, use or disclosure is permitted without the written permission of this firm. All Rights Reserved.
 The liability and control of the project are solely the responsibility of the client and associated documentation prior to the commencement of any building works. No liability is accepted for any errors or omissions in the project.
 Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they represent the intent of the project.
 In the event of these drawings or associated documentation, please contact this firm for advice or alteration.
 Design Vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless they do not comply with local safety standards or specifications.
 All works are to comply with the following:
 - The Building Code of Australia
 - The Queensland Building Act
 - All relevant Australian Standards
 - All relevant Local Authority By Laws and Planning Schemes.
 10.000 SCALE THIS DRAWING. IF IN DOUBT, ASK.
 TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

title:
 SITE PLAN (STAGE 1)

no.	amend.	date
D	DA ISSUE	19/05/15
C	TOWN PLANNER REQ. CHANGES	27/02/15
B	TOWN PLANNER REQ. CHANGES	24/02/15
A	L.C.C. REQUESTED CHANGES	19/02/15

scale:	As indicated	job no:	10018
date:	01/11/12	sheet no:	DA-1.02
drawn:	104	amendment:	D
checked:	SK	sheet size:	A1

WELLINGTON STREET

CANAL STREET

CHARLES STREET

CIMITIERE STREET

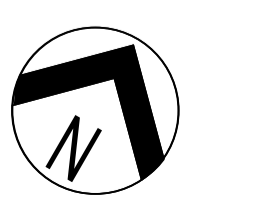
PROPOSED DEVELOPMENT

Condition 21
 Minimum extent of
 access easement

Minimum dimension
 1.5m from wall

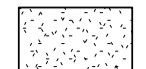


1 SITE PLAN
 DA-1.02 SCALE: 1:250

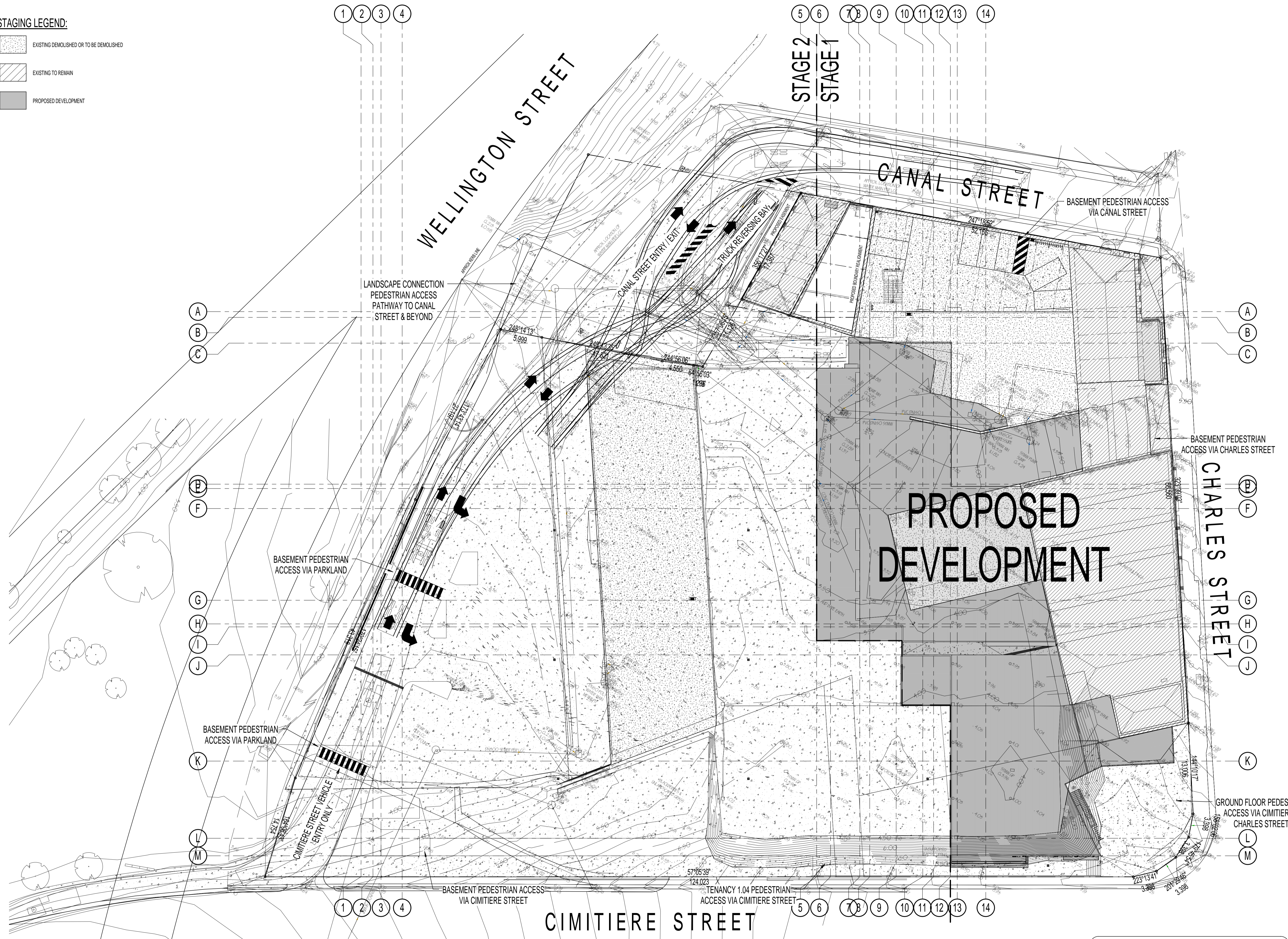
PRELIMINARY
 NOT FOR CONSTRUCTION



1:000 @ 5m
 1:200 @ 5m
 1:100 @ 5m

STAGING LEGEND:

-  EXISTING DEMOLISHED OR TO BE DEMOLISHED
-  EXISTING TO REMAIN
-  PROPOSED DEVELOPMENT



1 STAGING & LOCALITY PLAN
DA-1.02 SCALE: 1:250

PRELIMINARY
NOT FOR CONSTRUCTION

architectural building design



design.vibe
Pty. Ltd.

ACN 102 812 231 /
ABN 13 102 812 231
CRICOS A.C. No. 1902723
VIC. Reg. No. DP AD 36611 /
QPS 102 8642
TAS. Reg. No. CC 66627

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 No. 401 Luchun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:

This design documentation remains the sole property of Copyright © design.vibe Pty. Ltd. No reproduction, storage or disclosure is permitted without the written permission of this firm. All Rights Reserved.

The liability and/or responsibility for the design and/or construction of any building and associated documentation is the responsibility of the client and not the design.vibe Pty. Ltd. The design.vibe Pty. Ltd. is not responsible for any building or construction that does not comply with local authority or other approved documents.

Commencement of any works relating to these plans and documentation deems that the recipient of these plans and documentation has accepted and agreed to the design.vibe Pty. Ltd. design.vibe Pty. Ltd. is not responsible for any building or construction that does not comply with local authority or other approved documents.

If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.

design.vibe Pty. Ltd. will not be held responsible for the liability of contractor or project services unless they do not comply with local authority or other approved documents.

All works are to comply with the following:

- The Building Code of Australia
- The Queensland Building Act
- All relevant Australian Standards
- All relevant Local Authority By Laws and Planning Schemes.

DO NOT SCALE THIS DRAWING, IF IN DOUBT, ASK.
TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
BRILE PTY. LTD.

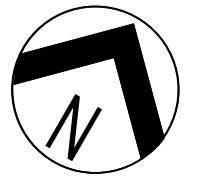
project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITIERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

title:
STAGING, EXTENT OF EXISTING &
LOCALITY PLAN (STAGE 1)

no.	amend.	date
D	DA ISSUE	19/05/15
C	TOWN PLANNER REQ. CHANGES	27/02/15
B	TOWN PLANNER REQ. CHANGES	24/02/15
A	L.C.C. REQUESTED CHANGES	19/02/15

scale:	As indicated	job no:	10018
date:	07/22/14	sheet no:	DA-1.03
drawn:	181	amendment:	D
checked:	SK	sheet size:	A1



1:250 @ A1
1:200 @ A2
1:100 @ B1

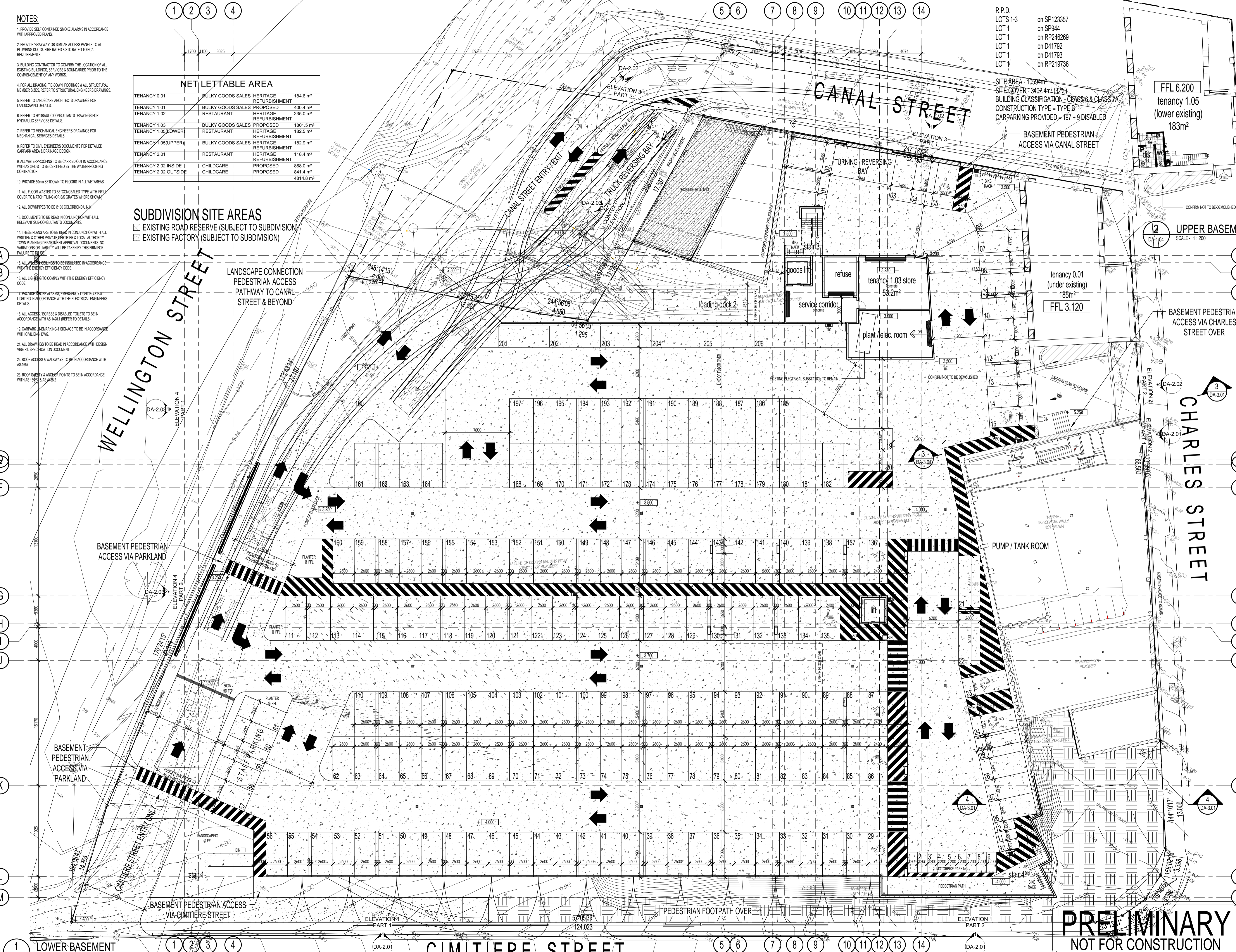
- NOTES:**
1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
 2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS, FIRE RATED & STIC RATED TO BCA REQUIREMENTS.
 3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
 4. FOR ALL BRACING, TIE-DOWNS, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
 5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
 6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
 7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
 8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
 9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3740 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
 10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL WET AREAS.
 11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH NEW COVER TO MATCH TILING (OR SS GRATES WHERE SHOWING).
 12. ALL DOWNPIPES TO BE Ø100 COLORBOND UNLID.
 13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB-CONSULTANTS DOCUMENTS.
 14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFIER & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR FAILURE TO DO SO.
 15. ALL WORKS ARE TO BE IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
 16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
 17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
 18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
 19. CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
 21. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE PL SPECIFICATION DOCUMENT.
 22. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1657.
 23. ROOF SHEET & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1562 & AS 4682.

NET LETTABLE AREA

TENANCY 0.01	BULKY GOODS SALES	HERITAGE REFURBISHMENT	184.6 m ²
TENANCY 1.01	BULKY GOODS SALES	PROPOSED	400.4 m ²
TENANCY 1.02	RESTAURANT	HERITAGE REFURBISHMENT	235.0 m ²
TENANCY 1.03	BULKY GOODS SALES	PROPOSED	1801.5 m ²
TENANCY 1.05(LOWER)	RESTAURANT	HERITAGE REFURBISHMENT	182.5 m ²
TENANCY 1.05(UPPER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.9 m ²
TENANCY 2.01	RESTAURANT	HERITAGE REFURBISHMENT	118.4 m ²
TENANCY 2.02 INSIDE	CHILDCARE	PROPOSED	868.0 m ²
TENANCY 2.02 OUTSIDE	CHILDCARE	PROPOSED	841.4 m ²
			4814.8 m ²

SUBDIVISION SITE AREAS

- EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
- EXISTING FACTORY (SUBJECT TO SUBDIVISION)



R.P.D.
LOTS 1-3
LOT 1 on SP123357
LOT 1 on SP944
LOT 1 on RP246269
LOT 1 on D41792
LOT 1 on D41793
LOT 1 on RP219736

SITE AREA - 10594m²
SITE COVER - 3402.4m² (32%)
BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
CONSTRUCTION TYPE = TYPE B
CARPARKING PROVIDED = 197 + 9 DISABLED

FFL 6.200
tenancy 1.05
(lower existing)
183m²

BASEMENT PEDESTRIAN ACCESS VIA CANAL STREET

FFL 3.120
tenancy 1.03 store
53.2m²

FFL 3.120
tenancy 0.01
(under existing)
185m²

BASEMENT PEDESTRIAN ACCESS VIA CHARLES STREET OVER

design vibe Pty. Ltd.
architectural building design

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Beiluxiang
5 NO. 401 Lushan, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:

This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction and/or disclosure are permitted without the written permission of this firm. All Rights Reserved.

The liability and/or responsibility for any errors or omissions in this plan and associated documentation rests with the client. The client is responsible for the accuracy and completeness of any building information and data provided to this firm. The client is responsible for the accuracy and completeness of any building information and data provided to this firm. The client is responsible for the accuracy and completeness of any building information and data provided to this firm.

Comments of any nature relating to these plans and documentation shall be the responsibility of the client. The client is responsible for the accuracy and completeness of any building information and data provided to this firm. The client is responsible for the accuracy and completeness of any building information and data provided to this firm.

All works are to comply with the following:

- The Building Code of Australia.
- The Queensland Building Act.
- All relevant Australian Standards.
- All relevant Local Authority By-Laws and Planning Schemes.

Design Vibe Pty. Ltd. will not be responsible for the liability of any contractor or subcontractor who is engaged by the client. The client is responsible for the accuracy and completeness of any building information and data provided to this firm. The client is responsible for the accuracy and completeness of any building information and data provided to this firm.

All works are to comply with the following:

- The Building Code of Australia.
- The Queensland Building Act.
- All relevant Australian Standards.
- All relevant Local Authority By-Laws and Planning Schemes.

Scale: 1:200

client:
BRILE PTY. LTD.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITIERE STREET
LAUNCESTON

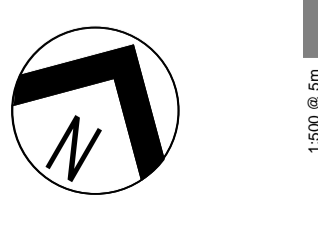
consultants:
IRENEE PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

title:
LOWER BASEMENT FLOOR PLAN
(STAGE 1)

no.	amend.	date
T	DA ISSUE	19/05/15
S	ADDED BIKE PARKING	13/05/15
R	COUNCIL REQUESTED CHANGES	28/04/15
Q	REMOVED CARPARKING	24/04/15
P	ADDED CARPARKING	15/04/15
O	LOADING DOCK / TRAFFIC	23/03/15
N	TOWN PLANNER REQ. CHANGES	27/02/15
M	TOWN PLANNER REQ. CHANGES	24/02/15
L	L.C.C. REQUESTED CHANGES	19/02/15
K	TOWN PLANNER REQ. CHANGES	01/12/14
J	TOWN PLANNER REQ. CHANGES	10/11/14
I	SERVICE CORRIDOR	21/10/14
H	STAGING AMENDMENT 2	20/10/14

scale: As indicated job no: 10018
date: 01/12/15 sheet no: DA-104
drawn: 104 amendment: 1
checked: SK sheet size: A1

PRELIMINARY
NOT FOR CONSTRUCTION



NOTES:

1. PROVIDE SELF-CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. REFER TO BRANNING OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS. FIRE RATED & STC RATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, THE DOWN FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3740 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL WET AREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH INFL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN).
12. ALL DOWNPIPES TO BE Ø100 COLORBOND UL90.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUBCONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN OTHER PRIVATE CERTIFIER & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR WORK TO DO SO.
15. ALL WALLS & CEILING TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1411 (REFER TO DETAILS).
19. CARPARK LINEMARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
20. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE PL SPECIFICATION DOCUMENT.
21. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1687.
22. ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1891.1 & AS 4882.

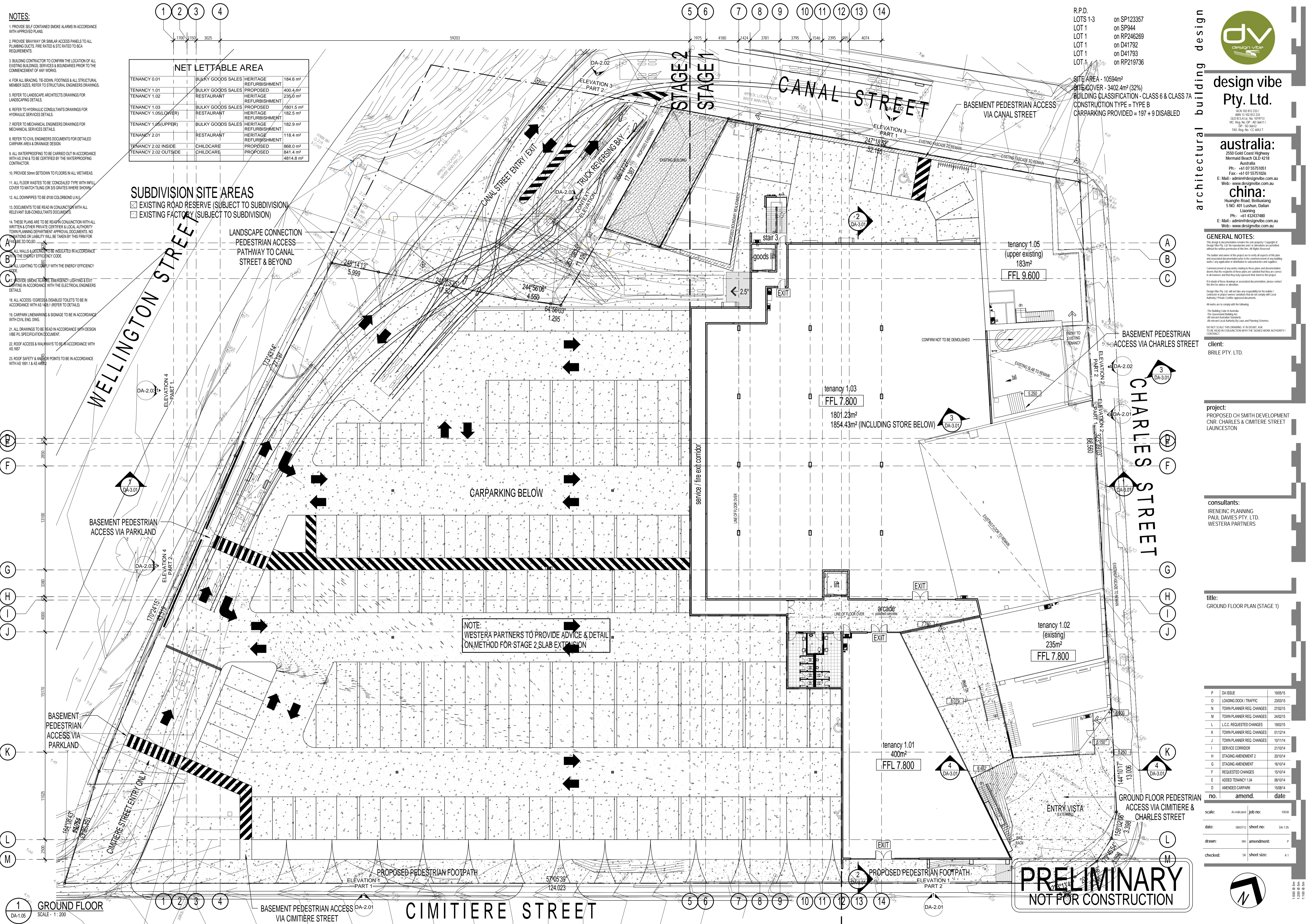
NET LETTABLE AREA

TENANCY 0.01	BULKY GOODS SALES	HERITAGE REFURBISHMENT	184.6 m ²
TENANCY 1.01	BULKY GOODS SALES	PROPOSED	400.4 m ²
TENANCY 1.02	RESTAURANT	HERITAGE REFURBISHMENT	235.0 m ²
TENANCY 1.03	BULKY GOODS SALES	PROPOSED	1801.23 m ²
TENANCY 1.05(LOWER)	RESTAURANT	HERITAGE REFURBISHMENT	182.5 m ²
TENANCY 1.05(UPPER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.9 m ²
TENANCY 2.01	RESTAURANT	HERITAGE REFURBISHMENT	118.4 m ²
TENANCY 2.02 INSIDE	CHILDCARE	PROPOSED	868.0 m ²
TENANCY 2.02 OUTSIDE	CHILDCARE	PROPOSED	841.4 m ²
			4814.8 m ²

SUBDIVISION SITE AREAS

- EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
- EXISTING FACTORY (SUBJECT TO SUBDIVISION)

LANDSCAPE CONNECTION
PEDESTRIAN ACCESS
PATHWAY TO CANAL
STREET & BEYOND



NOTE:
WESTERA PARTNERS TO PROVIDE ADVICE & DETAIL
ON METHOD FOR STAGE 2 SLAB EXTENSION

R.P.D.
LOTS 1-3 on SP123357
LOT 1 on SP944
LOT 1 on RP246269
LOT 1 on D41792
LOT 1 on D41793
LOT 1 on RP129736

SITE AREA - 10594m²
SITE COVER - 3402.4m² (32%)
BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
CONSTRUCTION TYPE - TYPE B
CARPARKING PROVIDED = 197 + 9 DISABLED

design vibe Pty. Ltd.
1201 102 812 231 / 102 812 233
CH 85 5 42 1000773
VIC. Reg. No. EP AD 36817
BN. 52 3642
TAS. Reg. No. CC 6627

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Beiliuxiang
5 NO. 401 Lushun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:
This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction, use or disclosure are permitted without the written permission of this firm. All Rights Reserved.
The designer and/or the project are not responsible for the building or project services unless they do not comply with local authority (public) certified requirements.
Comments of any works relating to these plans and documentation should be the responsibility of the client. They are not to be taken as an advisory or design service unless they do not comply with local authority (public) certified requirements.
All works are to comply with the following:
The Building Code of Australia
The Queensland Building Act
All relevant Australian Standards
All relevant Local Authority By Laws and Planning Schemes.
Design Vibe Pty. Ltd. will not be responsible for the building or project services unless they do not comply with local authority (public) certified requirements.
All works are to comply with the following:
The Building Code of Australia
The Queensland Building Act
All relevant Australian Standards
All relevant Local Authority By Laws and Planning Schemes.

client:
BRILE PTY. LTD.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITIERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

title:
GROUND FLOOR PLAN (STAGE 1)

no.	amend.	date
P	DA ISSUE	19/06/15
O	LOADING DOCK / TRAFFIC	23/03/15
N	TOWN PLANNER REQ. CHANGES	27/02/15
M	TOWN PLANNER REQ. CHANGES	24/02/15
L	L.C.C. REQUESTED CHANGES	19/02/15
K	TOWN PLANNER REQ. CHANGES	01/12/14
J	TOWN PLANNER REQ. CHANGES	10/11/14
I	SERVICE CORRIDOR	21/10/14
H	STAGING AMENDMENT 2	20/10/14
G	STAGING AMENDMENT	16/10/14
F	REQUESTED CHANGES	15/10/14
E	ADDED TENANCY 1.04	06/10/14
D	AMENDED CARPARK	15/08/14

scale: As indicated job no: 10038
date: 08/07/12 sheet no: DA-105
drawn: 101 amendment: P
checked: SK sheet size: A1

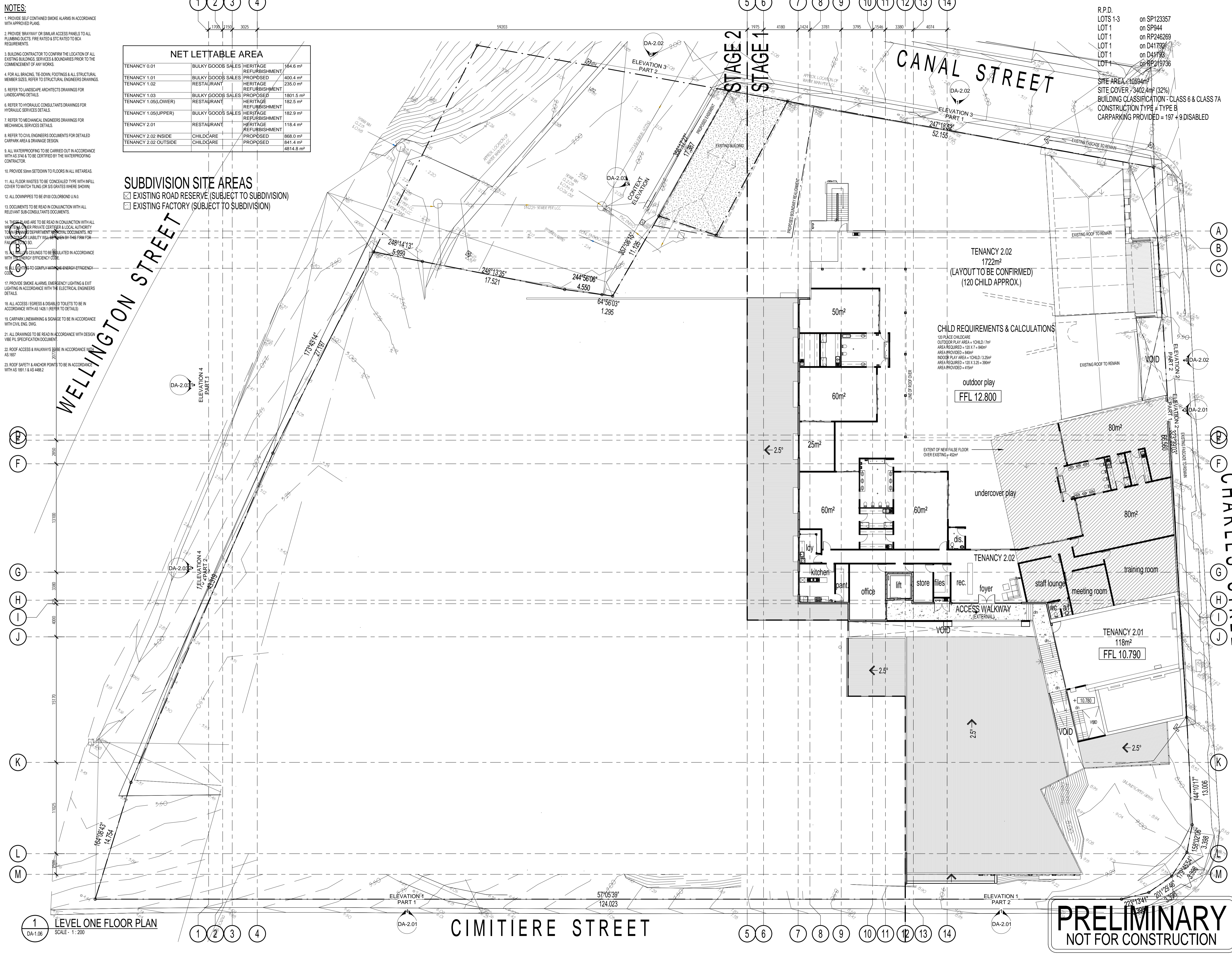
PRELIMINARY
NOT FOR CONSTRUCTION

NOTES:

1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS. FIRE RATED & STC RATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS SERVICES & DIMENSIONS PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, TIE-DOWN, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3540 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL VETAREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH INFILL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN).
12. ALL DOWNPIPES TO BE Ø100 COLORBOND U.N.O.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB CONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN OWNER PRIVATE CERTIFICATES & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT TECHNICAL DOCUMENTS. NO VARIATION LIABILITY WILL BE ASSUMED BY THIS FIRM FOR FAILURE TO DO SO.
15. ALL CEILING LIGHTS TO BE INSTALLED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL CEILING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
19. CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
21. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE PL SPECIFICATION DOCUMENT.
22. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1657.
23. ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1811.1 & AS 4488.2.

NET LETTABLE AREA			
TENANCY 0.01	BULKY GOODS SALES	HERITAGE REFURBISHMENT	184.6 m ²
TENANCY 1.01	BULKY GOODS SALES	PROPOSED RESTAURANT	400.4 m ²
TENANCY 1.02	BULKY GOODS SALES	PROPOSED RESTAURANT	235.0 m ²
TENANCY 1.03	BULKY GOODS SALES	PROPOSED RESTAURANT	1801.5 m ²
TENANCY 1.05(LOWER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.5 m ²
TENANCY 1.05(UPPER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.9 m ²
TENANCY 2.01	RESTAURANT	HERITAGE REFURBISHMENT	118.4 m ²
TENANCY 2.02 INSIDE	CHILD CARE	PROPOSED	868.0 m ²
TENANCY 2.02 OUTSIDE	CHILD CARE	PROPOSED	4814.8 m ²

SUBDIVISION SITE AREAS
 [] EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
 [] EXISTING FACTORY (SUBJECT TO SUBDIVISION)



R.P.D.
 LOTS 1-3 on SP123357
 LOT 1 on SP944
 LOT 1 on RP246269
 LOT 1 on D41792
 LOT 1 on D41793
 LOT 1 on RP219736

SITE AREA = 10594m²
 SITE COVER = 3402.4m² (32%)
 BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
 CONSTRUCTION TYPE = TYPE B
 CARPARKING PROVIDED = 197 + 9 DISABLED

architectural building design

design vibe Pty. Ltd.
 ACN 102 812 231 / ABN 13 102 812 231
 CHIEF EXECUTIVE OFFICER: PAUL DAVIES
 VIC. Reg. No. EP AD 36611 / QP 52 36412
 TAS. Reg. No. CC 6852 T

australia:
 2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Beiliuxiang
 5 NO. 401 Lushan, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This design & documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction, use or disclosure is permitted without the written permission of this firm. All Rights Reserved.
 The liability and control of the project are solely the responsibility of the client and associated documentation prior to the commencement of any building works or any application or discharge to accompany and require.
 Commencement of any works relating to these plans and documentation denotes that the recipient of these plans are advised that they are correct in all respects and that they fully represent the intent of the project.
 In the event of these drawings or associated documentation, please contact this firm for advice or alteration.
 Design Vibe Pty. Ltd. will not be held responsible for the liability of contractor or project services unless they do not comply with Local Authority / Public Officer approved documents.
 All works are to comply with the following:
 - The Building Code of Australia
 - The Queensland Building Act
 - All relevant Australian Standards
 - All relevant Local Authority By Laws and Planning Schemes.
 DESIGN VIBE PTY. LTD. WILL NOT BE HELD RESPONSIBLE FOR THE LIABILITY OF CONTRACTOR OR PROJECT SERVICES UNLESS THEY DO NOT COMPLY WITH LOCAL AUTHORITY / PUBLIC OFFICER APPROVED DOCUMENTS.
 TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

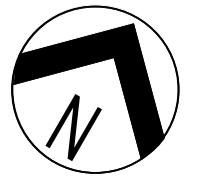
consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

title:
 LEVEL ONE FLOOR PLAN (STAGE 1)

no.	amend.	date
M	DA ISSUE	19/05/15
L	TOWN PLANNER REQ. CHANGES	27/02/15
K	TOWN PLANNER REQ. CHANGES	24/02/15
J	L.C.C. REQUESTED CHANGES	19/02/15
I	TOWN PLANNER REQ. CHANGES	01/12/14
H	TOWN PLANNER REQ. CHANGES	10/11/14
G	SERVICE CORRIDOR	21/10/14
F	STAGING AMENDMENT 2	20/10/14
E	STAGING AMENDMENT	16/10/14
D	REQUESTED CHANGES	15/10/14
C	ADDED TENANCY 1.04	06/10/14
B	TOWN PLANNER REQ. CHANGES	31/07/14
A	SKETCH ISSUE 1	25/07/14

scale: As indicated job no: 10018
 date: 06/09/12 sheet no: DA-1.06
 drawn: M amendment: M
 checked: SK sheet size: A1

PRELIMINARY
 NOT FOR CONSTRUCTION



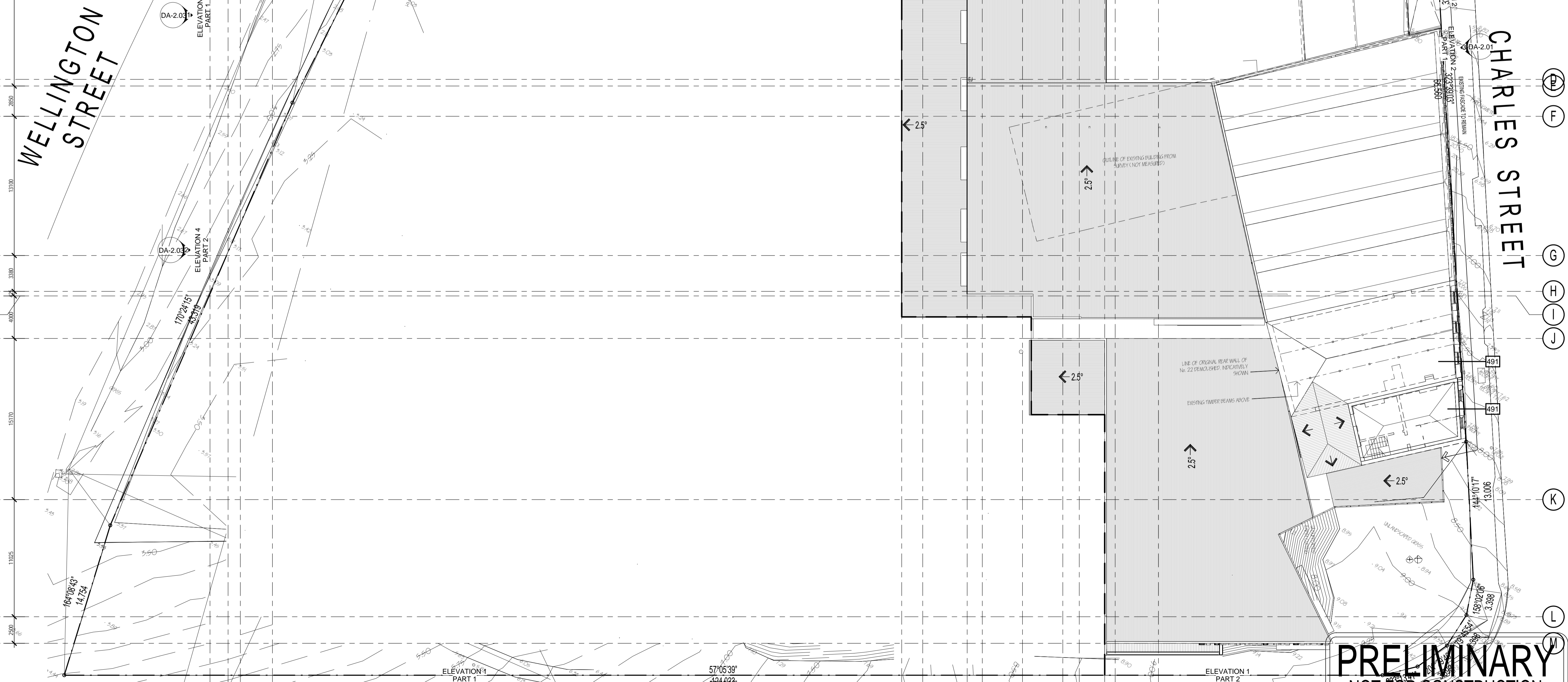
1:2000
 1:2000
 1:1000

NOTES:

1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS, FIRE RATED & SITUATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, TIE-DOWN FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3540 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL VETAREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH HULL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN).
12. ALL DOWNPIPES TO BE Ø100 COLORBOND U.N.O.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUBCONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFIER & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR FAILURE TO DO SO.
15. ALL WALLS & CEILINGS TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
19. CARPARK LINEMARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
20. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE P/L SPECIFICATION DOCUMENT.

NOTES	
020	COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC. TYP.)
178	FEATURE 'HEBEL' OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
485	INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
486	EXISTING OPENINGS INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
487	EXISTING WALL TO REMAIN
490	EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
491	EXISTING ROOF TO BE REINSTATED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
497	FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
501	FEATURE PLANTED GREEN WALL, ELMICH VGM OR SIMILAR TO MANUFACTURERS SPECIFICATION
503	SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
504	FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
505	EXISTING WINDOW TO BE INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
507	NEW WALL TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
511	ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
512	FEATURE GABION MATRESS
513	EPS SANDWICH WALL PANELS
515	HERITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (& THE PERMIT). FOR PROJECT NO 7-063 EX01A TO EX07A INCLUSIVE. HDAGIB TO HDAGIB INCLUSIVE, & HDAGIB TO HDAGIB, AND THE HERITAGE IMPACT STATEMENT DATED JUNE 2007, BY PAUL DAVIES PTY LTD.
516	TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERITAGE FACADE.

SUBDIVISION SITE AREAS
 [Symbol] EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
 [Symbol] EXISTING FACTORY (SUBJECT TO SUBDIVISION)



1 ROOF PLAN
 DA-1.07 SCALE: 1:200

PRELIMINARY
 NOT FOR CONSTRUCTION

architectural building design

design.vibe
Pty. Ltd.

ACN 102 812 231 /
 ABN 13 102 812 231
 CHS 5/4, St. No. 100/231
 VIC. Reg. No. EP AD 368117
 AFS 102 812 231
 TAS. Reg. No. CC 68271

australia:

2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:

Huanghe Road, Beiluxiang
 5 No. 401 Lushan, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:

This design documentation remains the sole property of design.vibe Pty. Ltd. No reproduction or distribution is permitted without the written permission of this firm. All Rights Reserved.

The liability and/or responsibility of this project are solely of design.vibe Pty. Ltd. and associated documentation is the commitment of any building contractor or project services unless they do not comply with local authority / public / client approved documents.

Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they fully represent the intent of the project.

In the event of any queries or associated documentation, please contact this firm for advice or alteration.

design.vibe Pty. Ltd. will not take any responsibility for the liability of contractor or project services unless they do not comply with local authority / public / client approved documents.

All works are to comply with the following:

- The Building Code of Australia
- The Queensland Building Act
- All relevant Australian Standards
- All relevant Local Authority By Laws and Planning Schemes.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

title:
 ROOF PLAN (STAGE 1)

no.	amend.	date
H	DA ISSUE	19/05/15
G	TOWN PLANNER REQ. CHANGES	24/02/15
F	L.C.C. REQUESTED CHANGES	19/02/15
E	TOWN PLANNER REQ. CHANGES	01/12/14
D	TOWN PLANNER REQ. CHANGES	10/11/14
C	STAGING AMENDMENT 2	20/10/14
B	STAGING AMENDMENT	16/10/14
A	SKETCH ISSUE 1	15/08/14

scale: As indicated job no: 10018

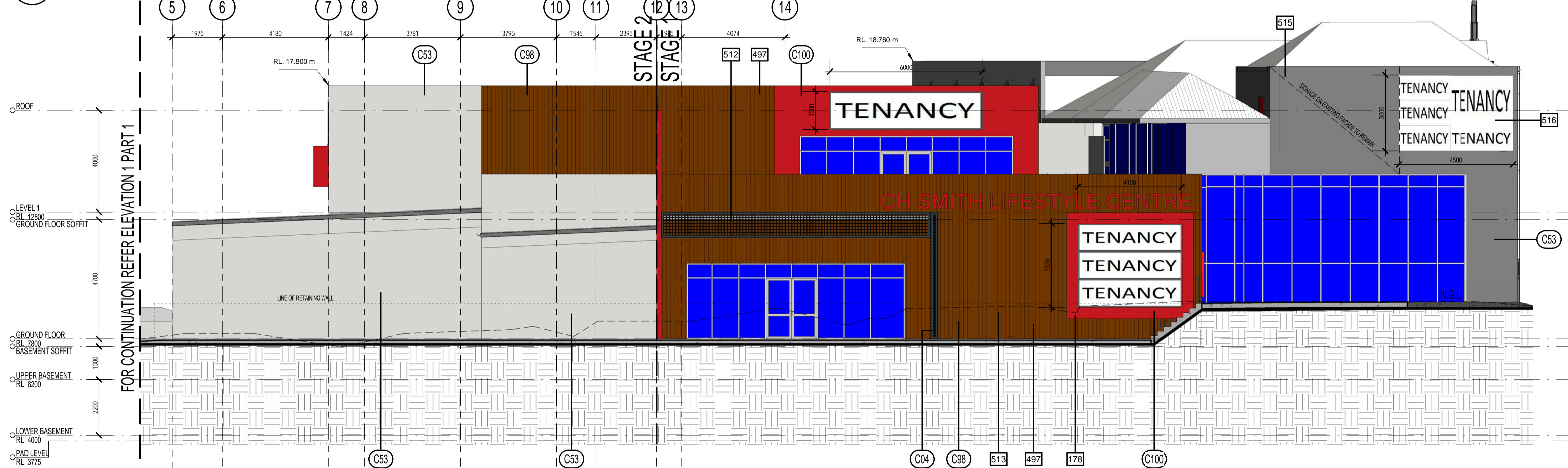
date: 09/10/12 sheet no: DA-1.07

drawn: 104 amendment: H

checked: SK sheet size: A1



4 ELEVATION 1 PART 1
DA-2.01 SCALE: 1:100



1 ELEVATION 1 PART 2
DA-2.01 SCALE: 1:100



3 ELEVATION 2 PART 1
DA-2.01 SCALE: 1:100



2 CIMITIERE STREET EXISTING FACADE
DA-2.01 SCALE - NTS

NOTES	
020	COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC TYP.)
178	FEATURE 'HEBEL' OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
485	INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
486	EXISTING OPENING INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
487	EXISTING WALL TO REMAIN
490	EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
491	EXISTING ROOF TO BE REINSTATED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
497	FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
501	FEATURE PLANTED GREEN WALL 'ELMIICH VGM' OR SIMILAR TO MANUFACTURERS SPECIFICATION
503	SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
504	FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
505	EXISTING WINDOW TO BE INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
507	NEW WALL TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
511	ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
512	FEATURE GABION MATTRESS.
513	EPS SANDWICH WALL PANELS.
515	HERITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (& THE PERMIT), FOR PROJECT NO 7-063-EX01A TO EX07A INCLUSIVE, HDA01B TO HDA04B INCLUSIVE, & HDA05B TO HDA09B, AND THE HERITAGE IMPACT STATEMENT DATED JUNE 2007, BY PAUL DAVIES PTY LTD.
516	TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERITAGE FACADE.

COLOUR LEGEND	
C04	RESENE 'MELTECA CHARCOAL GREY'
C07	RESENE 'AMBER GREY'
C53	RESENE 'WHITE'
C97	FEATURE STONE FINISH TO MATCH EXISTING HERITAGE FEATURES
C98	WESTERN RED CEDAR
C99	DULUX 'BLUE LOBELIA'
C100	SOLVER 'SIGNAL RED'

PRELIMINARY
NOT FOR CONSTRUCTION

architectural building design

design vibe Pty. Ltd.
ACN 102 812 231 / ABN 13 102 812 231
C/O B.S.A. Pty. Ltd. 10/202/23
VIC. Reg. No. 'DP' AD 36611 / (Ph: 52 36472)
TAS. Reg. No. CC 68527

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 NO. 401 Luchun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:
This design & documentation remains the sole property & Copyright of Design Vibe Pty. Ltd. No reproduction or distribution is permitted without the written permission of this firm. All Rights Reserved.
The holder and owner of the project are solely responsible for the plan and associated documentation prior to the commencement of any building works, any application or approval to government and suppliers.
Commencement of any works relating to these plans and documentation deems that the recipient of these plans are advised that they are correct in all respects and that they fully represent the intent of the project.
If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
Design Vibe Pty. Ltd. will not be held responsible for the liability of contractor or project services unless that do not comply with Local Authority/Heritage/Other approved documents.
All works are to comply with the following:
- The Building Code of Australia.
- The Queensland Building Act.
- All relevant Local Authority By Laws and Planning Schemes.
- All relevant Local Authority By Laws and Planning Schemes.
- All relevant Local Authority By Laws and Planning Schemes.
- All relevant Local Authority By Laws and Planning Schemes.
- All relevant Local Authority By Laws and Planning Schemes.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

Title:
ELEVATION SHEET 1 (STAGE 1)

no.	amend.	date
J	DA ISSUE	19/05/15
I	TOWN PLANNER REQ. CHANGES	27/02/15
H	TOWN PLANNER REQ. CHANGES	24/02/15
G	L.C.C. REQUESTED CHANGES	19/02/15
F	TOWN PLANNER REQ. CHANGES	01/12/14
E	TOWN PLANNER REQ. CHANGES	10/11/14
D	STAGING AMENDMENT 2	20/10/14
C	STAGING AMENDMENT	16/10/14
B	TOWN PLANNER REQ. CHANGES	31/07/14
A	SKETCH ISSUE 1	25/07/14

scale: 1:100 job no: 10018
date: 09/12/12 sheet no: DA-2.01
drawn: 104 amendment: J
checked: SK sheet size: A1

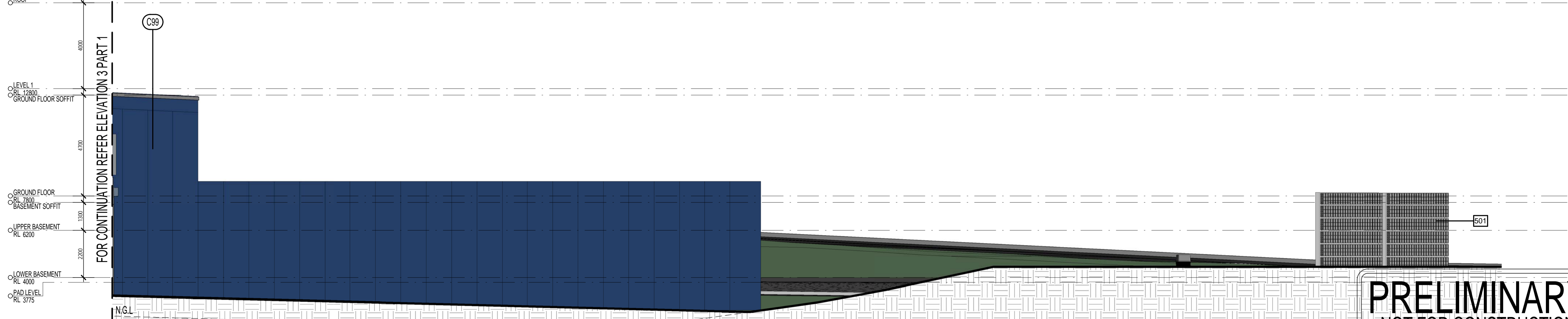
1:000 & 1:200 & 1:500 & 1:1000 & 1:2000



4 ELEVATION 2 PART 2
DA-2.02 SCALE - 1:100



1 ELEVATION 3 PART 1
DA-2.02 SCALE - 1:100



5 ELEVATION 3 PART 2
DA-2.02 SCALE - 1:100

NOTES

- 020 COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC. TYP.)
- 178 FEATURE HEBEL OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
- 485 INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 486 EXISTING OPENING INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 487 EXISTING WALL TO REMAIN
- 490 EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 491 EXISTING ROOF TO BE REINSTATED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 497 FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
- 501 FEATURE PLANTED GREEN WALL 'ELMICH VGM' OR SIMILAR TO MANUFACTURERS SPECIFICATION
- 503 SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
- 504 FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
- 505 EXISTING WINDOW TO BE INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 507 NEW WALL TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 511 ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 512 FEATURE GAGON MATRESSES.
- 513 EPS SANDWICH WALL PANELS.
- 515 HERITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (& THC PERMIT). FOR PROJECT NO 7-063 EX01A TO EX07A INCLUSIVE. HDMSB TO HDMSB INCLUSIVE, & HDMSB TO HDMSB; AND THE HERITAGE IMPACT STATEMENT DATED JUNE 2007, BY PAUL DAVIES PTY LTD.
- 516 TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERITAGE FACADE.

COLOUR LEGEND

C04	RESENE 'MELTECA CHARCOAL GREY'
C07	RESENE 'AMBER GREY'
C53	RESENE 'WHITE'
C97	FEATURE STONE FINISH TO MATCH EXISTING HERITAGE FEATURES
C88	WESTERN RED CEDAR
C99	DULUX 'BLUE LOBELIA'
C100	SOLVER 'SIGNAL RED'

GENERAL NOTES:
This design & documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction or disclosure is permitted without the written permission of this firm. All Rights Reserved.
The builder and owner of the project are to verify all aspects of this plan and associated documentation prior to the commencement of any building works or any application or submission to government and suppliers.
Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they have reviewed the same for the project.
If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
Design Vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless they do not comply with Local Authority/Trade/Contract approved documents.
All works are to comply with the following:
The Building Code of Australia.
The Queensland Building Act.
All relevant Australian Standards.
All relevant Local Authority By Laws and Planning Schemes.
SERVED SCALE: THIS DRAWING, IF IN SEVERAL JOBS, TO BE READ IN CONJUNCTION WITH THE SCALED WORK AUTHORITY / CONTRACT.

architectural building design

design vibe Pty. Ltd.
AUSTRALIA:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au
CHINA:
Huanghe Road, Beiliuxiang
5 NO. 401 Lushun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

client:
BRILE PTY. LTD.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

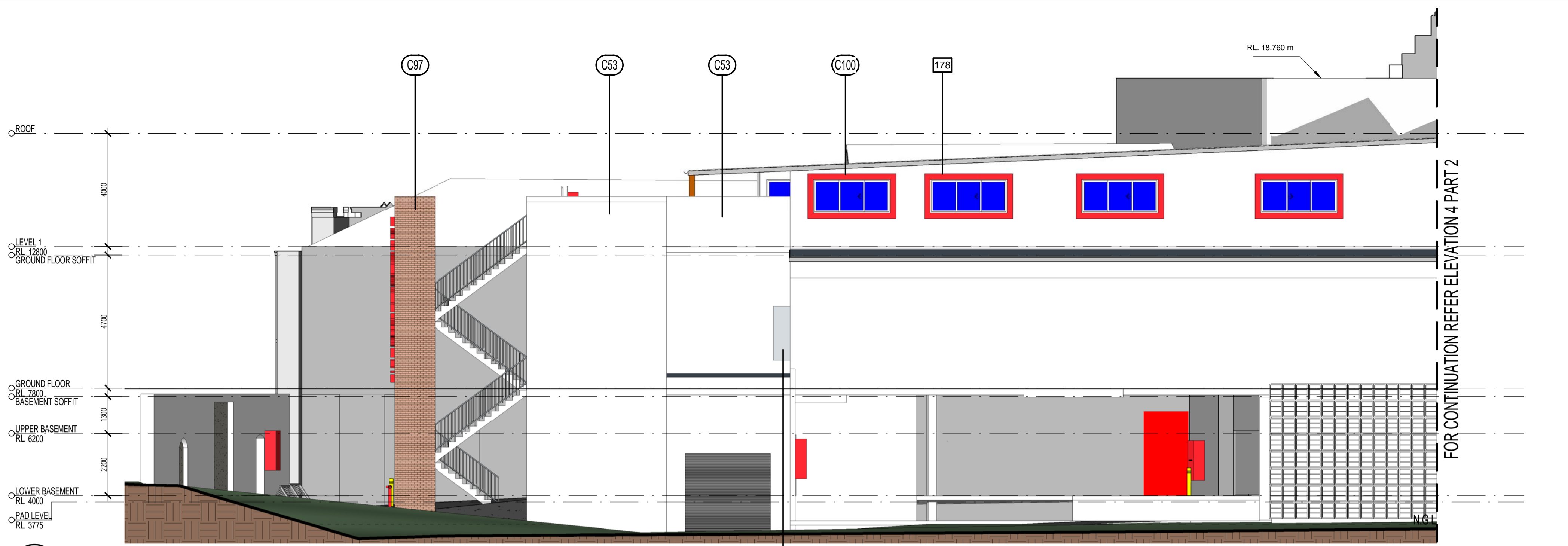
title:
ELEVATION SHEET 2 (STAGE 1)

no.	amend.	date
J	DA ISSUE	19/05/15
I	TOWN PLANNER REQ. CHANGES	27/02/15
H	TOWN PLANNER REQ. CHANGES	24/02/15
G	L.C.C. REQUESTED CHANGES	19/02/15
F	TOWN PLANNER REQ. CHANGES	01/12/14
E	TOWN PLANNER REQ. CHANGES	10/11/14
D	STAGING AMENDMENT 2	20/10/14
C	STAGING AMENDMENT	16/10/14
B	TOWN PLANNER REQ. CHANGES	31/07/14
A	SKETCH ISSUE 1	25/07/14

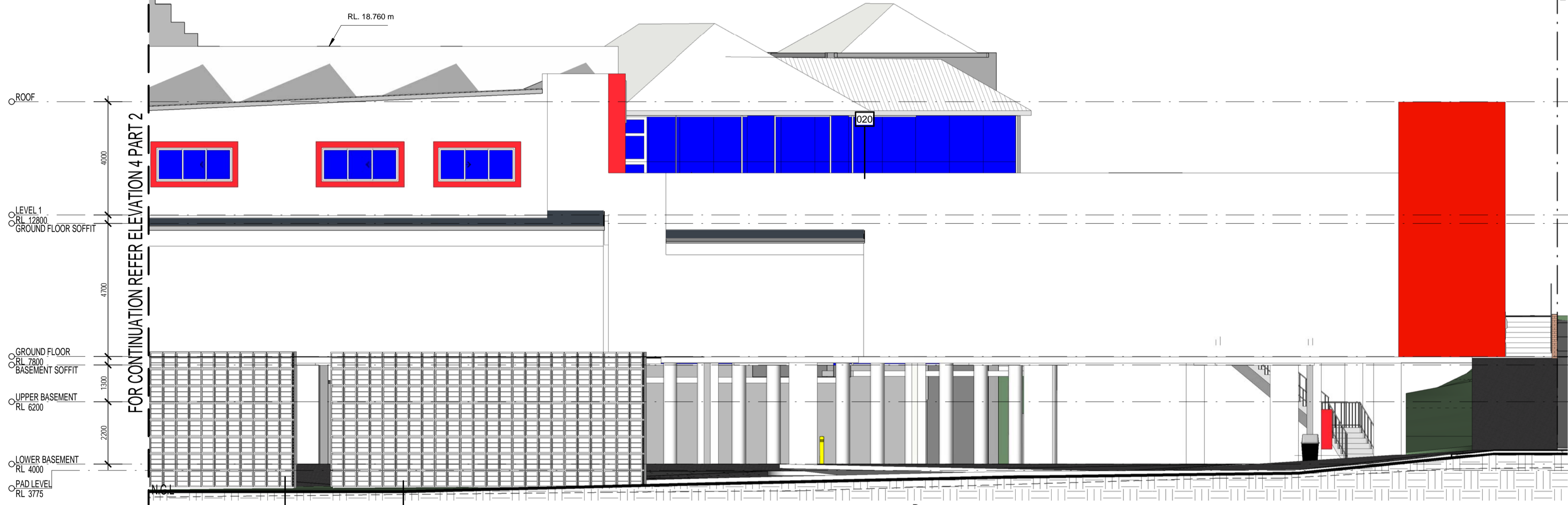
scale:	1:100	job no:	10018
date:	09/10/12	sheet no:	DA-2.02
drawn:	SK	amendment:	J
checked:	SK	sheet size:	A1

PRELIMINARY
NOT FOR CONSTRUCTION

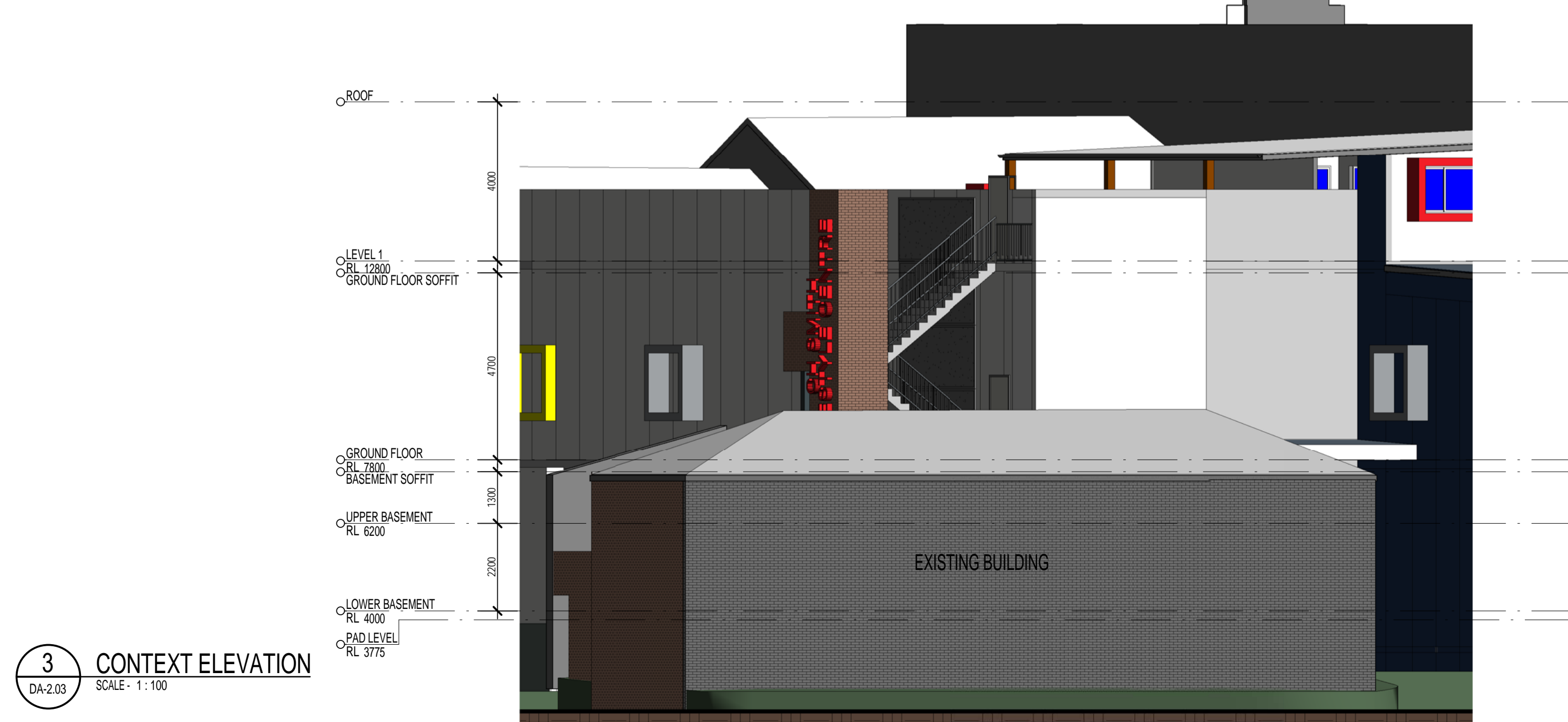
1:500 @ 5m
1:200 @ 5m
1:100 @ 5m



1 ELEVATION 4 PART 1
DA-2.03 SCALE: 1:100



2 ELEVATION 4 PART 2
DA-2.03 SCALE: 1:100



3 CONTEXT ELEVATION
DA-2.03 SCALE: 1:100

NOTES	
020	COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC.TYP.)
178	FEATURE 'HEBEL' OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
485	INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
486	EXISTING OPENING INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
487	EXISTING WALL TO REMAIN
490	EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
491	EXISTING ROOF TO BE REINSTATE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
497	FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
501	FEATURE PLANTED GREEN WALL ELNICH VGM OR SIMILAR TO MANUFACTURERS SPECIFICATION
503	SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
504	FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
505	EXISTING WINDOW TO BE INFILLED TO HERRITAGE CONSULTANTS DETAILS (TYPICAL)
507	NEW WALL TO HERRITAGE CONSULTANTS DETAILS (TYPICAL)
511	ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
512	FEATURE GABION MATTRESS.
513	EPS SANDWICH WALL PANELS.
515	HERRITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (& THE PERMIT), FOR PROJECT NO 7-063-EX01A TO EX07A INCLUSIVE, HD4015 TO HD4045 INCLUSIVE, & HD4055 TO HD4065, AND THE HERRITAGE IMPACT STATEMENT DATED JUNE 2007, BY PAUL DAVIES PTY LTD.
516	TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERRITAGE FACADE.

COLOUR LEGEND	
C04	RESENE 'MELTEGA CHARCOAL GREY'
C07	RESENE 'AMBER GREY'
C53	RESENE 'WHITE'
C97	FEATURE STONE FINISH TO MATCH EXISTING HERITAGE FEATURES
C98	WESTERN RED CEDAR
C99	DULUX 'BLUE LOBELIA'
C100	SOLVER 'SIGNAL RED'

architectural building design

design vibe Pty. Ltd.
ACN 102 812 231 / ABN 13 102 812 231
CLD 5.5.16. No. 190273
VIC. Reg. No. DP AD 36611 / EP 152 8412
TAS. Reg. No. CC 6621

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 NO. 401 Luchun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:
This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction and/or disclosure are permitted without the written permission of this firm. All Rights Reserved.
The liability and/or responsibility of this project are solely of the client and associated documentation prior to the commencement of any building works, any application or submission to government and regulators.
Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they fully represent the intent of the project.
If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
Design Vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless that do not comply with Local Authority / Professional Engineer requirements.
All works are to comply with the following:
The Building Code of Australia.
The Queensland Building Act.
All relevant Australian Standards.
All relevant Local Authority By Laws and Planning Schemes.
DESIGNER'S SCALE: THIS DRAWING, IF IN SEVERAL JOBS, TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
BRILE PTY. LTD.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

no.	amend.	date
F	DA ISSUE	19/05/15
E	TOWN PLANNER REQ. CHANGES	27/02/15
D	L.C.C. REQUESTED CHANGES	19/02/15
C	TOWN PLANNER REQ. CHANGES	01/12/14
B	TOWN PLANNER REQ. CHANGES	10/11/14
A	STAGING AMENDMENT 2	20/10/14

title:
ELEVATION SHEET 3 (STAGE 1)

no.	amend.	date
F	DA ISSUE	19/05/15
E	TOWN PLANNER REQ. CHANGES	27/02/15
D	L.C.C. REQUESTED CHANGES	19/02/15
C	TOWN PLANNER REQ. CHANGES	01/12/14
B	TOWN PLANNER REQ. CHANGES	10/11/14
A	STAGING AMENDMENT 2	20/10/14

no.	amend.	date
F	DA ISSUE	19/05/15
E	TOWN PLANNER REQ. CHANGES	27/02/15
D	L.C.C. REQUESTED CHANGES	19/02/15
C	TOWN PLANNER REQ. CHANGES	01/12/14
B	TOWN PLANNER REQ. CHANGES	10/11/14
A	STAGING AMENDMENT 2	20/10/14

no.	amend.	date
F	DA ISSUE	19/05/15
E	TOWN PLANNER REQ. CHANGES	27/02/15
D	L.C.C. REQUESTED CHANGES	19/02/15
C	TOWN PLANNER REQ. CHANGES	01/12/14
B	TOWN PLANNER REQ. CHANGES	10/11/14
A	STAGING AMENDMENT 2	20/10/14

scale:	1:100	job no:	10018
date:	10/10/14	sheet no:	DA-2.03
drawn:	184	amendment:	F
checked:	SK	sheet size:	A1

PRELIMINARY
NOT FOR CONSTRUCTION



architectural building design

design.vibe Pty. Ltd.

ACN 102 812 231 / ABN 13 102 812 231
CLD 54.14.16. No. 1000273
VIC. Reg. No. DP AD 36611 /
QPL 52 36412
TAS. Reg. No. CC 6662 T

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E: Mail - admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 NO. 401 Luchun, Dalian
Liaoning
Ph: +61 432437480
E: Mail - admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:
This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction and/or disclosure are permitted without the written permission of this firm. All Rights Reserved.
The holder and owner of the project are to verify all aspects of this plan and associated documentation prior to the commencement of any building works or any application or discharge to any authority and to sign-off.
Commencement of any works relating to these plans and documentation deems that the sign-off of these plans are sufficient that they are correct in all respects and that they fully represent the intent of the project.
If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
Design Vibe Pty. Ltd. will take no responsibility for the liability of contractor or project services unless that do not comply with Local Authority / Professional approved documents.
All works are to comply with the following:
The Building Code of Australia
The Queensland Building Act
All relevant Australian Standards
All relevant Local Authority By Laws and Planning Schemes.
DO NOT SCALE THIS DOCUMENT, IF IN DOUBT, ASK TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
BRILE PTY. LTD.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

title:
SECTIONS (STAGE 1)

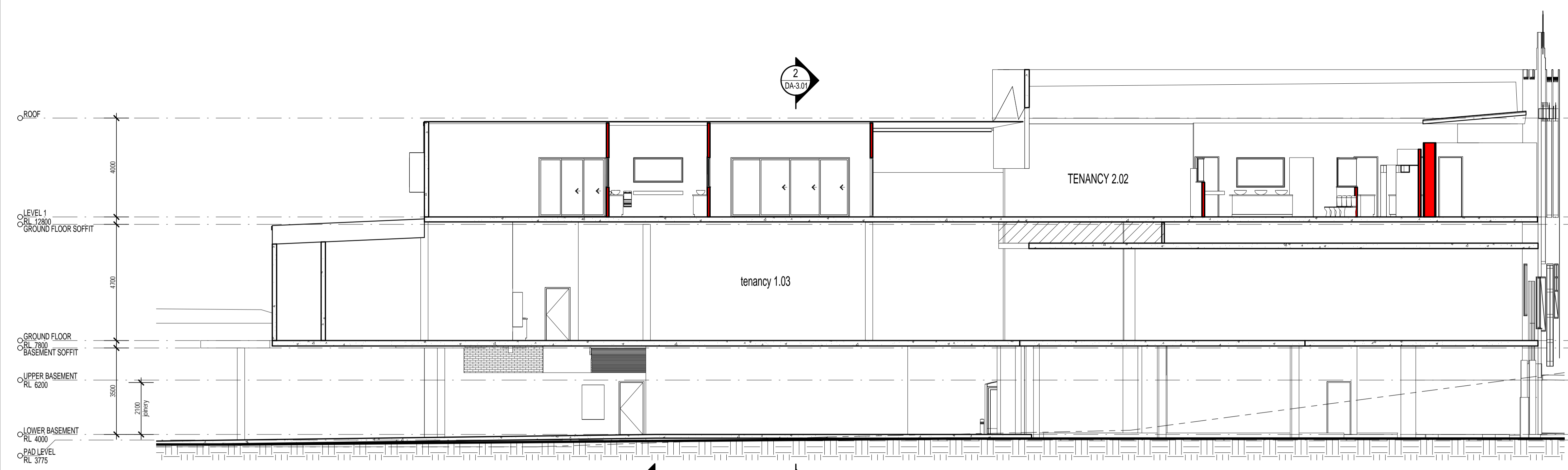
no.	amend.	date
D	DA ISSUE	19/05/15
C	ADDED PEDESTRIAN SECTIONS	13/05/15
B	L.C.C. REQUESTED CHANGES	19/02/15
A	SKETCH ISSUE 1	22/08/14

scale: 1:100 job no: 10018
date: 09/10/12 sheet no: DA-3.01
drawn: 104 amendment: D
checked: SK sheet size: A1

INTERNAL LININGS:
CEILINGS - SQUARE-SET ALL PLASTERBOARD WALL / CEILING JUNCTIONS (TYP.)
REFER TO MECHANICAL ENGINEERS DRAWINGS FOR LOCATION OF ACCESS PANELS & AIRCONDITIONING / MECHANICAL VENTILATION REGISTERS.
13mm PLASTERBOARD LINING SCREW-FIXED TO US OF FLOOR TRUSSES / CEILING FRAMING - SEL. PAINT FINISH (TYP.)
WALLS - 10mm PLASTERBOARD LINING TO INTERNAL STUD WALLS (VILLBOARD TO WET AREAS) - SEL. PAINT FINISH (TYP.)
PACK OUT WALL LINING FROM CONC. TILT-UP PANELS TO ACCOMMODATE PLUMBING WHERE REQUIRED TO WET AREAS (TYP.)
ALL WALLS & CEILINGS TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE
DOORS - ALL INTERNAL DOORS TO BE AS PER DOOR SCHEDULE

**PRELIMINARY
NOT FOR CONSTRUCTION**

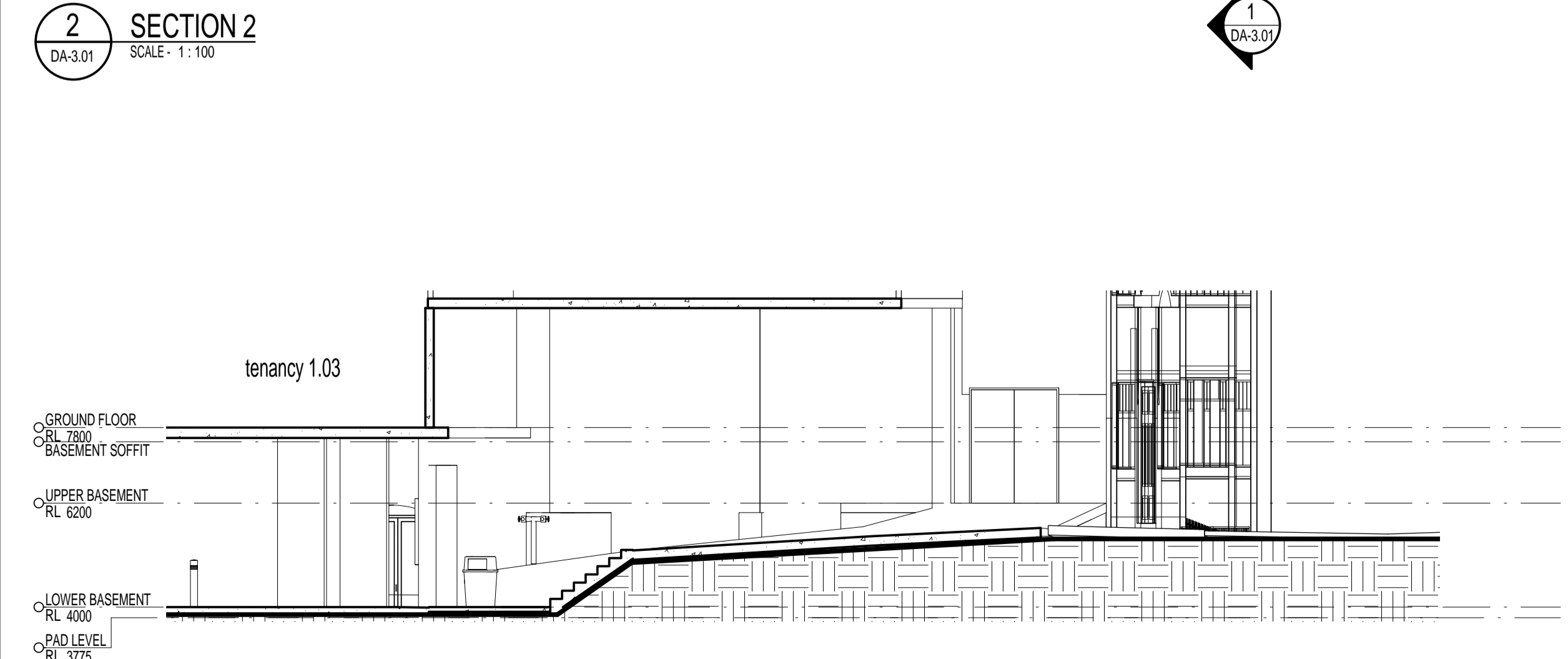
1:500 @ 50mm
1:200 @ 50mm
1:100 @ 50mm



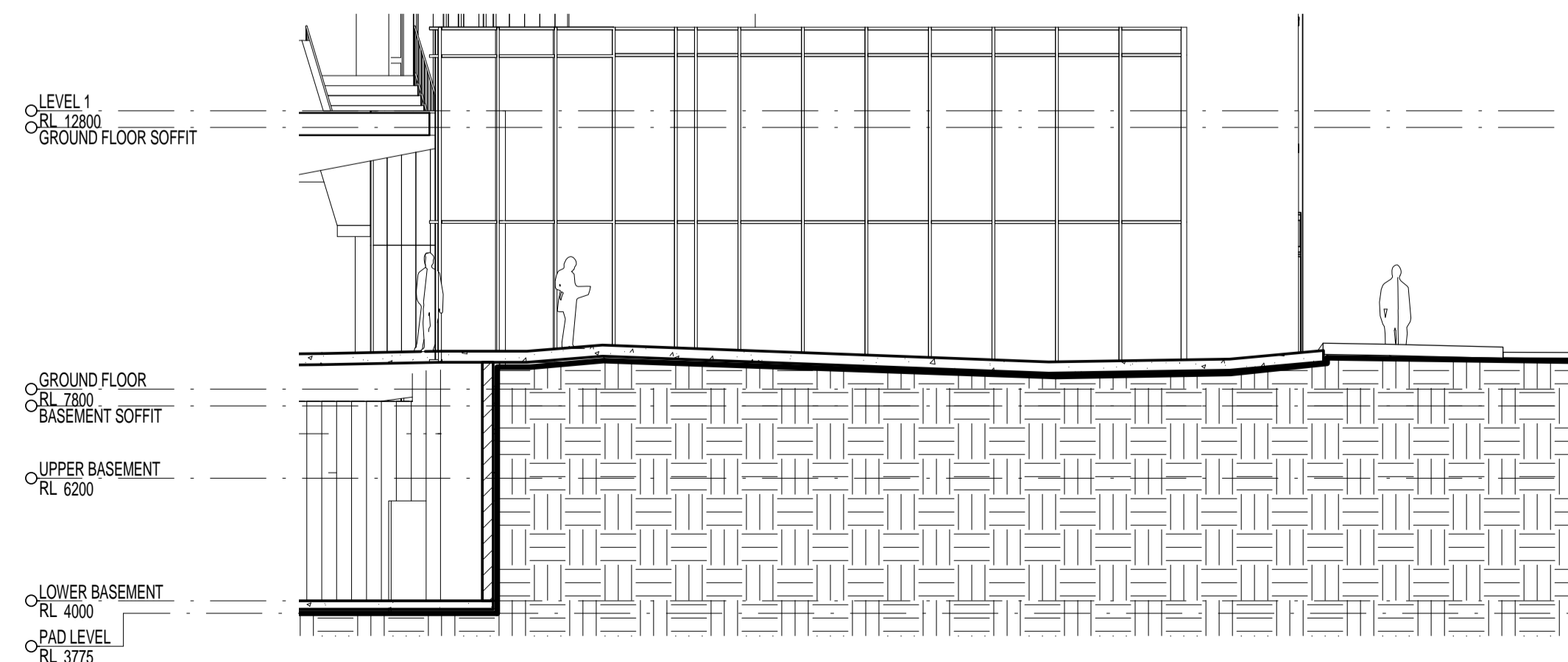
1 SECTION 1
DA-3.01 SCALE: 1:100



2 SECTION 2
DA-3.01 SCALE: 1:100



3 CHARLES STREET PEDESTRIAN ACCESS
DA-3.01 SCALE: 1:100



4 CORNER OF CHARLES & CIMITERO STREET PEDESTRIAN ACCESS
DA-3.01 SCALE: 1:100

NOTES:

- 1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS. FIRE RATED & STC RATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, TIE-DOWN, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3740 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL WET AREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH IN-FILL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN).
12. ALL DOWNPIPES TO BE @100 COLORBOND U.N.O.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB-CONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFICATES & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR FAILURE TO DO SO.
15. ALL WALLS & CEILING TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
19. CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
21. ALL DRAWINGS TO BE READ IN CONJUNCTION WITH DESIGN VIBE PL. SPECIFICATION DOCUMENT.
22. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1657.
23. ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1891.1 & AS 4882.

LEGEND:

Table with 2 columns: Item code and Description. Includes items like AC, AL, AP, APP, AS, AVIN, BGA, BO, BR, BS, BT, CAV, CBD, CONC, CS, CTS, DN, DRY, DP, DW, E, FC, FD, FE, FFL, FG, FH, FHR, FSL, FW, GB, GP, HC, HD, HP, HVS, K & C, LC, LV, M, MTSO, MV, MW.

TYPICAL WALL TYPES:

Table showing wall types and their construction details. Includes 90mm timber/steel stud wall, laminated toilet partition, 190mm concrete blockwork, 250mm double masonry brick wall, 190mm concrete blockwork, 150mm reinforced concrete tilt-up wall panel, 175mm reinforced concrete tilt-up wall panel, 250mm reinforced concrete column, 300mm reinforced concrete column, 350mm reinforced concrete column, 190mm concrete blockwork, and site measured curtain wall.

ENERGY PROVISIONS

- EXTERNAL WALLS NEED AN OVERALL RATING OF R2.8.
EXTERNAL ROOF / CEILING NEEDS AN OVERALL RATING OF R3.2.
A MONITORING SYSTEM FOR CARPARKING A SUSPENDED CEILING ACHIEVES R1.50 INSULATION THAT LEAST WILL NEED TO BE ADDED (OR SIMILY PLANK UNDER THE REFLECTIVE SARKING).
EXTERNAL CONCRETE WALLS WITH CAVITY AND PLASTERBOARD ACHIEVE R0.4 SO ADD WALL INSULATION. CAVITY IS NEEDED TO COMPRESS INSULATION TO BE INCREASED SO AS NOT TO COMPRESS INSULATION.
EXTERNAL CURTAIN WEIGHTS TO FULLY ACHIEVE R0.4 SO WALL INSULATION OF R2.5 WILL BE NEEDED FOR THESE.
THESE DRAWINGS ARE TO BE READ IN ACCORDANCE WITH McUTOCHON & MACDONALD ENERGY EFFICIENCY REPORT.

PRELIMINARY NOT FOR CONSTRUCTION

Table with 3 columns: PART OF BUILDING, CLASS, APPROX FLOOR AREA. Includes RETAIL / SALES (Class 6, 11,406 m²) and CARPARKING (Class 7a, 5,378 m²).

SERVICES LEGEND

Table of service symbols and descriptions. Includes AC SPLIT SYSTEM CONDENSER, AC SPLIT SYSTEM HEAD, AC CONTROL PANEL, STEEL BOLLARD, SOLAR PANEL, FIRE CONTROL PANEL, ELECTRICAL METER BOX / DISTRIBUTION BOARD, SMOKE DETECTOR, SMOKE ALARMS TO COMPLY WITH B.C.A. CLASS 1 & 10, SPLIT 2.2 & WITH A.S.3786, SQUARE HOLLOW SECTION POST, HARDWOOD TAMBER POST, EXIT SIGN, EXIT ARROW, EXTERIOR LIGHTPOLE, SUB BOARD, MECH VENTILATION, MECHANICAL VENTILATION REQUIRED WHERE INDICATED, MINIMUM EXHAUST AIR REQUIREMENTS BASED ON USE OF ENCLOSURE AS PER AS 1688.2 1991-APPENDIX B, LAUNDRY - 20LS WIRED TO LIGHT SWITCH, BATH, WC, ENSUITE - 20LS WIRED TO LIGHTSWITCH.

CONSTRUCTION TO COMPLY WITH:

- BCA CLASS 1 & 10 & CLASS 2-9
AS 3700
AS 3600
AS 1250 / AS 1538 / AS 4100
AS 2327
AS 1684
AS 1694
AS 3740
AS 1288
AS 1694 / AS 2057
AS 2179.1
AS 2049 / AS 2050
AS 1552.1
AS 3786
AS 1428.1
AS 3500
AS / NZS 2293
AS 2419
AS 2441
AS 1684
AS / NZS 3013
AS / NZS 2904
AS / NZS 1688
AS 2047
AS 2870
AS 3740
AS 1428
AS 3700
AS 3600
AS 1250 / AS 1538 / AS 4100
AS 2327
AS 1684
AS 1694
AS 3740
AS 1288
AS 1694 / AS 2057
AS 2179.1
AS 2049 / AS 2050
AS 1552.1
AS 3786
AS 1428.1
AS 3500
AS / NZS 2293
AS 2419
AS 2441
AS 1684
AS / NZS 3013
AS / NZS 2904
AS / NZS 1688
AS 2047
AS 2870
AS 3740
AS 1428

MIN. FIRE RESISTANCE LEVEL (FRL) REQUIREMENTS: CLASS 6

Table with 2 columns: BUILDING ELEMENT and MIN. FIRE. Lists requirements for external walls, internal walls, columns, and roof construction.

MIN. FIRE RESISTANCE LEVEL (FRL) REQUIREMENTS: CLASS 7a

Table with 2 columns: BUILDING ELEMENT and MIN. FIRE. Lists requirements for external walls, internal walls, columns, and roof construction.

MIN. FIRE RESISTANCE LEVEL (FRL) REQUIREMENTS: CARPARKS

Table with 2 columns: BUILDING ELEMENT and MIN. FIRE. Lists requirements for external walls, internal walls, columns, beams, and roof construction.

PROPOSED CH SMITH COMMERCIAL DEVELOPMENT CNR. CHARLES & CIMITIERE STREET LAUNCESTON (STAGE 2)



R.P.D. LOTS 1-3 on SP123357
LOT 1 on SP944
LOT 1 on RP246269
LOT 1 on D41792
LOT 1 on D41793
LOT 1 on RP219736

SITE AREA - 10594m²
SITE COVER - 7605m² (72%)
BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
CONSTRUCTION TYPE = TYPE B
CARPARKING PROVIDED = 151 + 9 DISABLED

Table titled SITE COVER showing TOTAL SITE COVER (7605.2 m²) and another value (7605.2 m²).

Table titled NET LETTABLE AREA showing tenancy breakdown by type and area.

Table titled GROSS FLOOR AREA showing area breakdown by level and room type.

Table titled ACCESS WALKWAY showing area breakdown by level and room type.

Table titled LANDSCAPING showing area breakdown by landscaping area.

Table titled DRAWING SCHEDULE showing drawing number and title.



design.vibe
Pty. Ltd.

ACN 102 812 231 /
ABN 13 102 812 231
CRICOS A.C. No. 1029723
VIC. Reg. No. DP AD 36611 /
EP 102 812 231
TAS. Reg. No. CC 6852 T

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 NO. 401 Luchun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:
This design & documentation remains the sole property of Copyright © design.vibe Pty. Ltd. No reproduction, use or disclosure is permitted without the written permission of this firm. All Rights Reserved.
The holder of these plans is responsible for the accuracy of the plan and associated documentation prior to the commencement of any building works. Any application or discharge to documentation and registers.
Commencement of any works relating to these plans and documentation deems that the holder of these plans is satisfied that they are correct in all respects and that they represent the state of the project.
If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
design.vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless that do comply with local Authority / Public Officer approved documents.
All works are to comply with the following:
The Building Code of Australia
The Queensland Building Act
All relevant Australian Standards
All relevant Local Authority By Laws and Planning Schemes.
DO NOT SCALE THIS DRAWING. IF IN DOUBT, ASK TO BE IN CONSTRUCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
BRILE PTY. LTD.

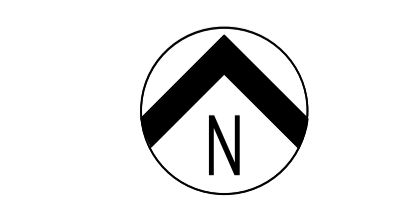
project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMIERE STREET
LAUNCESTON

consultants:

Title:
LOCALITY PLAN (STAGE 2)

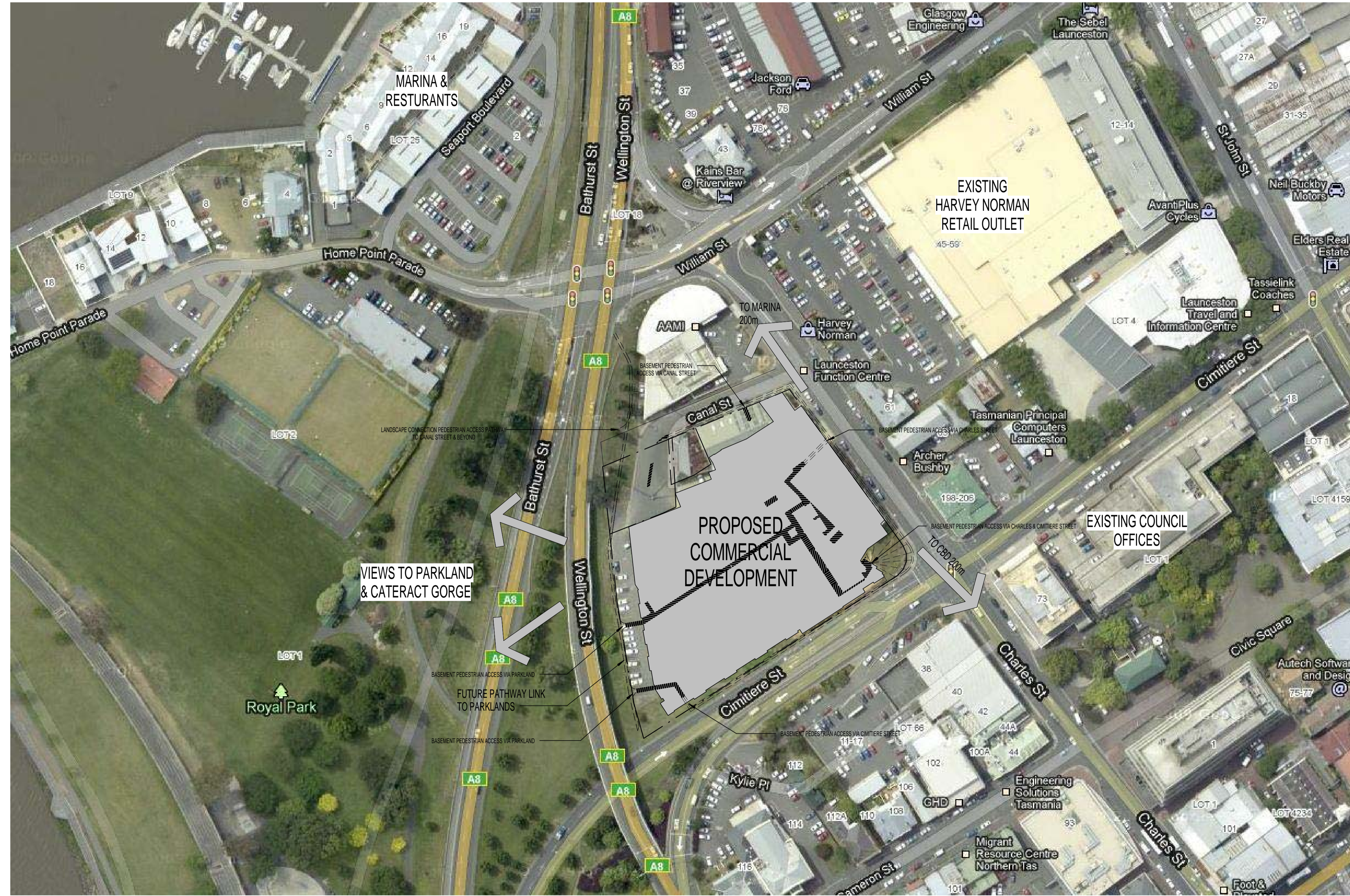
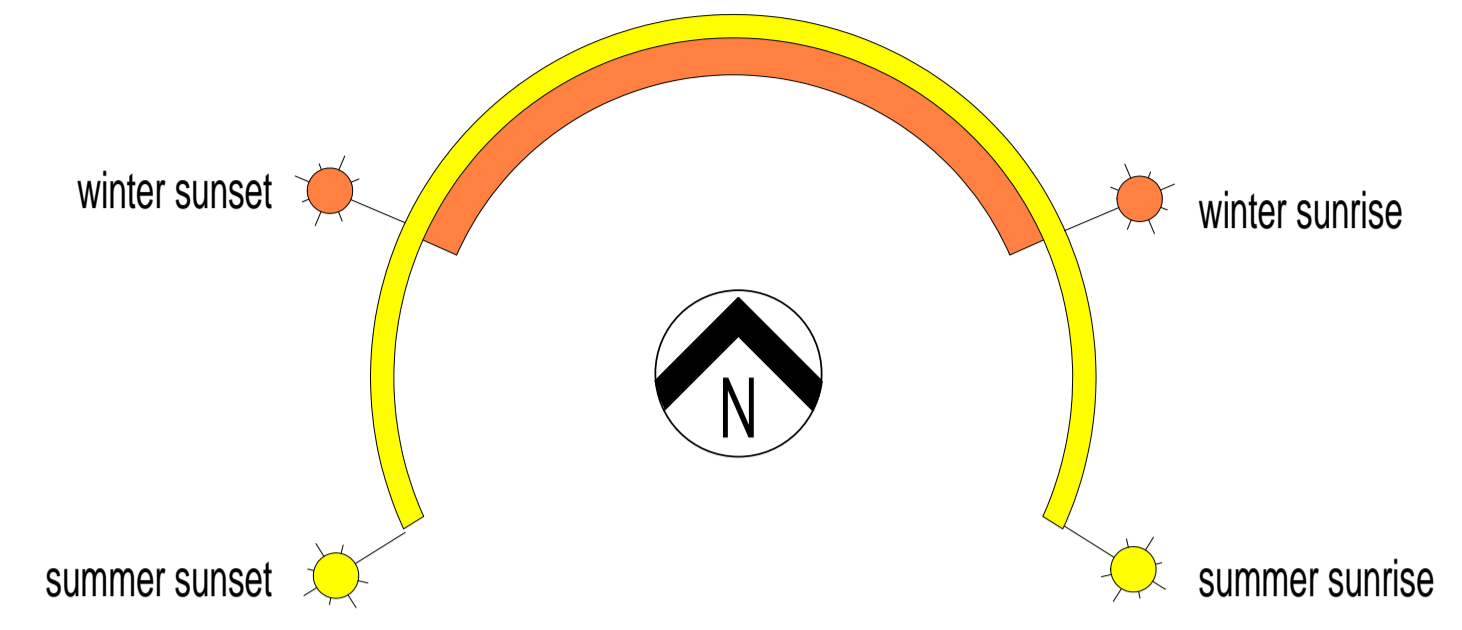
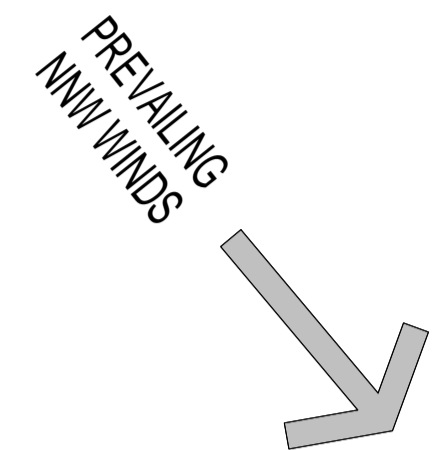
no.	amend.	date
B	DA ISSUE	19/05/15
A	L.C.C. REQUESTED CHANGES	18/03/15

scale: 1:1000 job no: 10018
date: 09/11/15 sheet no: DA-101
drawn: 184 amendment: B
checked: SK sheet size: A1



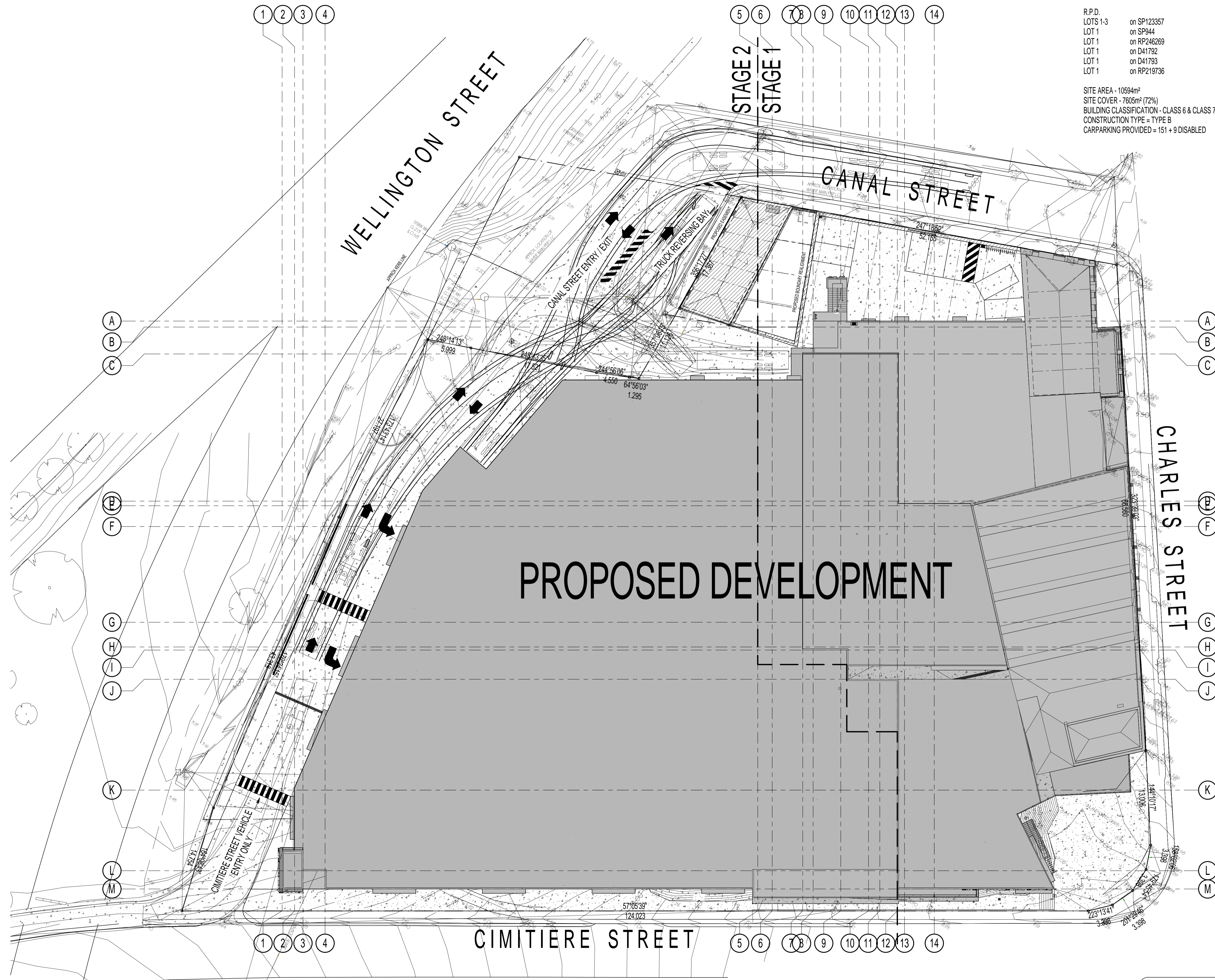
1:000 B 5m
1:200 B 5m
1:100 B 5m

===== INDICATES PEDESTRIAN ACCESS THROUGH DEVELOPMENT, WITH FUTURE LANDSCAPED FEATURE LINK TO ADJACENT PARKLANDS



1 LOCALITY PLAN
SCALE: 1:1000
DA-1.01

PRELIMINARY
NOT FOR CONSTRUCTION



R.P.D.
 LOTS 1-3 on SP123357
 LOT 1 on SP944
 LOT 1 on RP246269
 LOT 1 on D41792
 LOT 1 on D41792
 LOT 1 on RP219736

SITE AREA - 10594m²
 SITE COVER - 7605m² (72%)
 BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
 CONSTRUCTION TYPE = TYPE B
 CARPARKING PROVIDED = 151 + 9 DISABLED

- NOTES:**
1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
 2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS. FIRE RATED & STCRATED TO BCA REQUIREMENTS.
 3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS SERVICES & SOUNDINGS PRIOR TO THE COMMENCEMENT OF ANY WORKS.
 4. FOR ALL BRACING, TIE-DOWN, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
 5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
 6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
 7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
 8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
 9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3540 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
 10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL WET AREAS.
 11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH INFILL COVERING TO MATCH TILING (OR S/S GRATES WHERE SHOWN)
 12. ALL DOWNPIPES TO BE 110X COLOREDOND LIND.
 13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB-CONSULTANTS DOCUMENTS.
 14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFIER & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR FAILURE TO DO SO.
 15. ALL WALLS & CEILINGS TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
 16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
 17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
 18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
 19. CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DIVG.
 21. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE P/L SPECIFICATION DOCUMENT.
 22. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1657
 23. ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1891.1 & AS 4482.

architectural building design



design vibe Pty. Ltd.
 ACN 102 812 231 / ABN 13 102 812 231
 CR. 0.5.5.16.16. No. 1002723
 VIC. Reg. No. 087 AD 36611 / 087 AD 36612
 TAS. Reg. No. CC 6852 T

australia:
 2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Belluxiang
 5 NO. 401 Luchun, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction or distribution is permitted without the written permission of this firm. All Rights Reserved.
 The liability and/or responsibility of this project is the responsibility of the client and associated documentation prior to the commencement of any building works. Any application or distribution to subcontractors and suppliers.
 Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they represent the intent of the project.
 In the event of these drawings or associated documentation, please contact this firm for advice or alteration.
 Design Vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless they do not comply with local safety / health codes or standards.
 All works are to comply with the following:
 - The Building Code of Australia
 - The Queensland Building Act
 - All relevant Australian Standards
 - All relevant Local Authority By Laws and Planning Schemes.
 100% NOT SCALE THIS DRAWING. IF IN DOUBT, ASK.
 TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

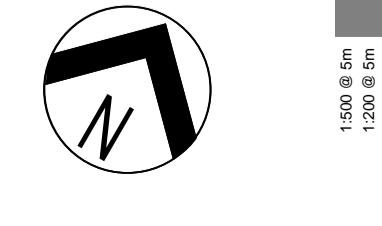
consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

1 SITE PLAN
 DA-1.02 SCALE: 1:250

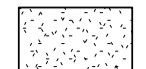


PRELIMINARY
 NOT FOR CONSTRUCTION

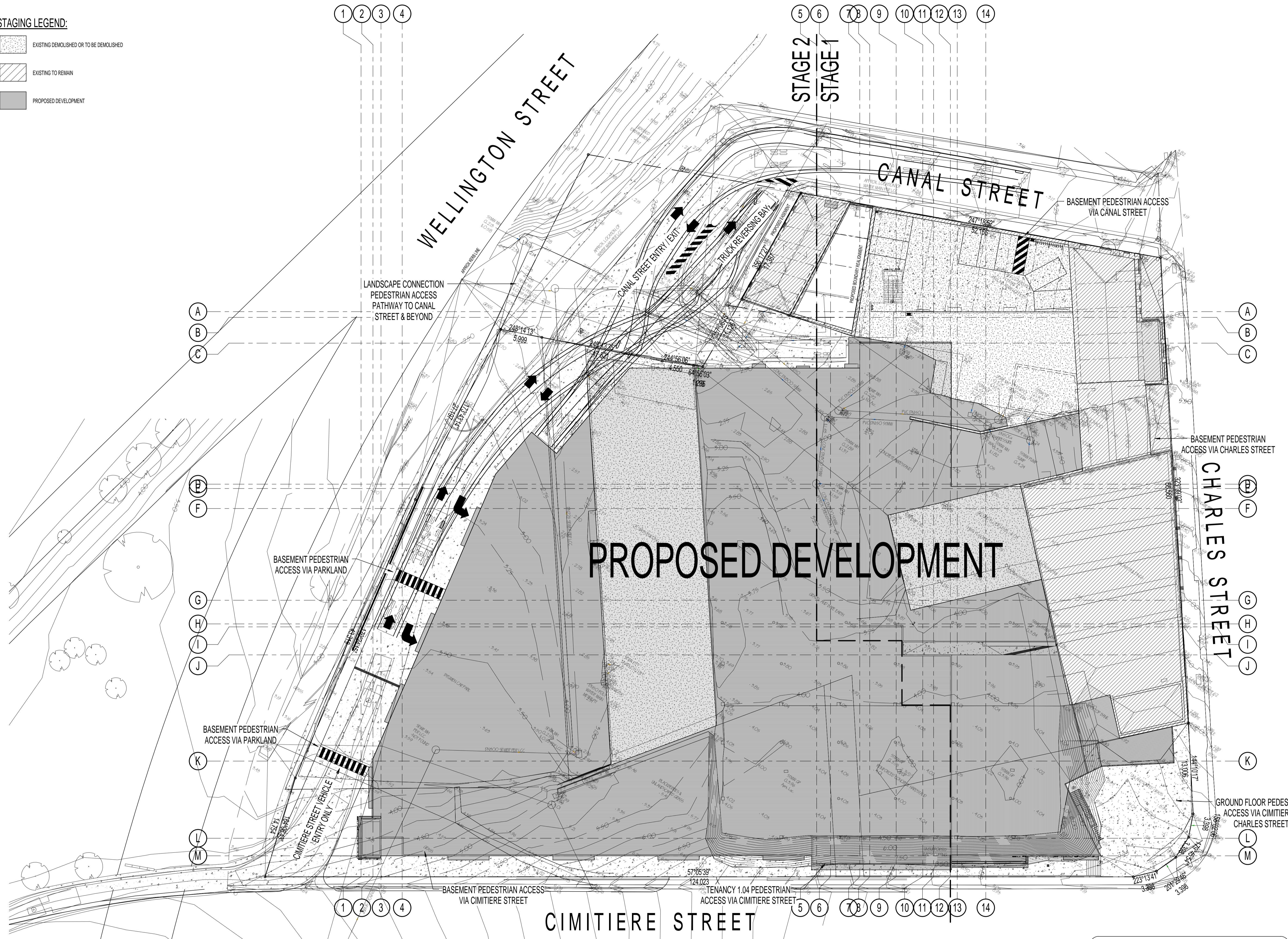
no.	amend.	date
D	DA ISSUE	19/05/15
C	TOWN PLANNER REQ. CHANGES	27/02/15
B	TOWN PLANNER REQ. CHANGES	24/02/15
A	L.C.C. REQUESTED CHANGES	19/02/15

scale:	As indicated	job no:	10018
date:	01/11/12	sheet no:	DA-1.02
drawn:	104	amendment:	D
checked:	SK	sheet size:	A1



STAGING LEGEND:

-  EXISTING DEMOLISHED OR TO BE DEMOLISHED
-  EXISTING TO REMAIN
-  PROPOSED DEVELOPMENT



1 STAGING & LOCALITY PLAN
DA-1.02 SCALE: 1:250

PRELIMINARY
NOT FOR CONSTRUCTION

architectural building design



design.vibe
Pty. Ltd.

ACN 102 812 231 /
ABN 13 102 812 231
CRICOS A.C. No. 1002213
VIC. Reg. No. DP AD 36611 /
QPS 102 8642
TAS. Reg. No. CC 66627

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 No. 401 Luchun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:

This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction, use or disclosure is permitted without the written permission of this firm. All Rights Reserved.

The liability and/or responsibility for the design of this plan and associated documentation rests with the client. The client is responsible for the accuracy of the information provided to the design team. The design team is not responsible for the accuracy of the information provided to the client.

Commencement of any works relating to these plans and documentation deems that the recipient of these plans and documentation has accepted the design and that they represent the client's requirements.

If in doubt of these drawings or associated documentation, please contact this firm for advice or clarification.

Design Vibe Pty. Ltd. will not be held responsible for the liability of contractor or project services unless they do not comply with Local Authority/Professional approved documents.

All works are to comply with the following:

- The Building Code of Australia
- The Queensland Building Act
- All relevant Australian Standards
- All relevant Local Authority By Laws and Planning Schemes.

DO NOT SCALE THIS DRAWING, IF IN DOUBT, ASK.
TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY CONTRACT.

client:
BRILE PTY. LTD.

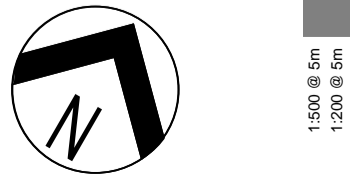
project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITIERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

title:
STAGING, EXTENT OF EXISTING &
LOCALITY PLAN (STAGE 2)

no.	amend.	date
D	DA ISSUE	19/05/15
C	TOWN PLANNER REQ. CHANGES	27/02/15
B	TOWN PLANNER REQ. CHANGES	24/02/15
A	L.C.C. REQUESTED CHANGES	19/02/15

scale:	As indicated	job no:	10018
date:	07/22/14	sheet no:	DA-1.03
drawn:	181	amendment:	D
checked:	SK	sheet size:	A1



1:250 @ A1
1:200 @ A2
1:100 @ A3

NOTES:

1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS, FIRE RATED & STIC RATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, TIE-DOWNS, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3740 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL WET AREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH NEW COVER TO MATCH TILING (OR SIS GRATES WHERE SHOWING).
12. ALL DOWNPIPES TO BE Ø100 COLORBOND ULN.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB-CONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE RESPECTIVE & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR FAILURE TO DO SO.
15. ALL WORKS DEEMED TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
19. CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
21. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE P.L. SPECIFICATION DOCUMENT.
22. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1687.
23. ROOF SHEET & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1861 & AS 4682.

NET LETTABLE AREA			
TENANCY 0.01	BULKY GOODS SALES	HERITAGE REFURBISHMENT	184.6 m ²
TENANCY 1.01	BULKY GOODS SALES	PROPOSED	400.4 m ²
TENANCY 1.02	RESTAURANT	HERITAGE REFURBISHMENT	235.0 m ²
TENANCY 1.03	BULKY GOODS SALES	PROPOSED	1801.5 m ²
TENANCY 1.04	BULKY GOODS SALES	PROPOSED	2500.2 m ²
TENANCY 1.04 LOWER STORE	BULKY GOODS SALES	PROPOSED	1115.9 m ²
TENANCY 1.04 UPPER STORE	BULKY GOODS SALES	PROPOSED	533.6 m ²
TENANCY 1.05 (LOWER)	RESTAURANT	HERITAGE REFURBISHMENT	182.5 m ²
TENANCY 1.05 (UPPER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.9 m ²
TENANCY 2.01	RESTAURANT	HERITAGE REFURBISHMENT	118.4 m ²
TENANCY 2.02 INSIDE	CHILD CARE	PROPOSED	968.0 m ²
TENANCY 2.02 OUTSIDE	CHILD CARE	PROPOSED	964.4 m ²
			9664.6 m ²

- SUBDIVISION SITE AREAS**
- EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
 - EXISTING FACTORY (SUBJECT TO SUBDIVISION)

R.P.D.
 LOTS 1-3 on SP123357
 LOT 1 on SP944
 LOT 1 on RP246269
 LOT 1 on D41792
 LOT 1 on D41793
 LOT 1 on RP219736

SITE AREA - 10594m²
 SITE COVER - 7665m² (72%)
 BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
 CONSTRUCTION TYPE = TYPE B
 CARPARKING PROVIDED = 151 + 9 DISABLED

FFL 6.200
 tenancy 1.05 (lower existing)
 183m²

BASEMENT PEDESTRIAN ACCESS VIA CANAL STREET

UPPER BASEMENT
 SCALE - 1:200

BASEMENT PEDESTRIAN ACCESS VIA CHARLES STREET OVER

GENERAL NOTES:

This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction or disclosure is permitted without the written permission of this firm. All Rights Reserved.

The liability and/or responsibility for any errors or omissions in this plan and associated documentation rests with the client. The client is responsible for the accuracy and completeness of any building information and that they may represent the client in this regard.

Comments of any nature relating to these plans and documentation shall be the responsibility of the client. The client is responsible for the accuracy and completeness of any building information and that they may represent the client in this regard.

All works are to comply with the following:

- The Building Code of Australia.
- The Queensland Building Act.
- All relevant Local Authority By-Laws and Planning Schemes.
- All relevant Australian Standards.

Design Vibe Pty. Ltd. will not be responsible for the liability of any contractor or subcontractor who does not comply with local authority (Council) approval requirements.

All works are to comply with the following:

The Building Code of Australia.
 The Queensland Building Act.
 All relevant Local Authority By-Laws and Planning Schemes.
 All relevant Australian Standards.

Scale: 1:200 (as shown) or as indicated on site.
 TO BE READ IN CONJUNCTION WITH THE SEaled WORK AUTHORITY CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

title:
 LOWER BASEMENT FLOOR PLAN
 (STAGE 2)

no.	amend.	date
T	DA ISSUE	19/05/15
S	ADDED BIKE PARKING	13/05/15
R	COUNCIL REQUESTED CHANGES	28/04/15
Q	REMOVED CARPARKING	24/04/15
P	ADDED CARPARKING	15/04/15
O	LOADING DOCK / TRAFFIC	23/03/15
N	TOWN PLANNER REQ. CHANGES	27/02/15
M	TOWN PLANNER REQ. CHANGES	24/02/15
L	L.C.C. REQUESTED CHANGES	19/02/15
K	TOWN PLANNER REQ. CHANGES	01/12/14
J	TOWN PLANNER REQ. CHANGES	10/11/14
I	SERVICE CORRIDOR	21/10/14
H	STAGING AMENDMENT 2	20/10/14

scale: As indicated job no: 10018
 date: 01/12/12 sheet no: DA-104
 drawn: 104 amendment: 1
 checked: SK sheet size: A1



design vibe Pty. Ltd.

australia:
 2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Belluxiang
 5 NO. 401 Lushan, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction or disclosure is permitted without the written permission of this firm. All Rights Reserved.

The liability and/or responsibility for any errors or omissions in this plan and associated documentation rests with the client. The client is responsible for the accuracy and completeness of any building information and that they may represent the client in this regard.

Comments of any nature relating to these plans and documentation shall be the responsibility of the client. The client is responsible for the accuracy and completeness of any building information and that they may represent the client in this regard.

All works are to comply with the following:

- The Building Code of Australia.
- The Queensland Building Act.
- All relevant Local Authority By-Laws and Planning Schemes.
- All relevant Australian Standards.

Design Vibe Pty. Ltd. will not be responsible for the liability of any contractor or subcontractor who does not comply with local authority (Council) approval requirements.

All works are to comply with the following:

The Building Code of Australia.
 The Queensland Building Act.
 All relevant Local Authority By-Laws and Planning Schemes.
 All relevant Australian Standards.

Scale: 1:200 (as shown) or as indicated on site.
 TO BE READ IN CONJUNCTION WITH THE SEaled WORK AUTHORITY CONTRACT.

client:
 BRILE PTY. LTD.

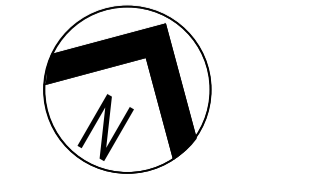
project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

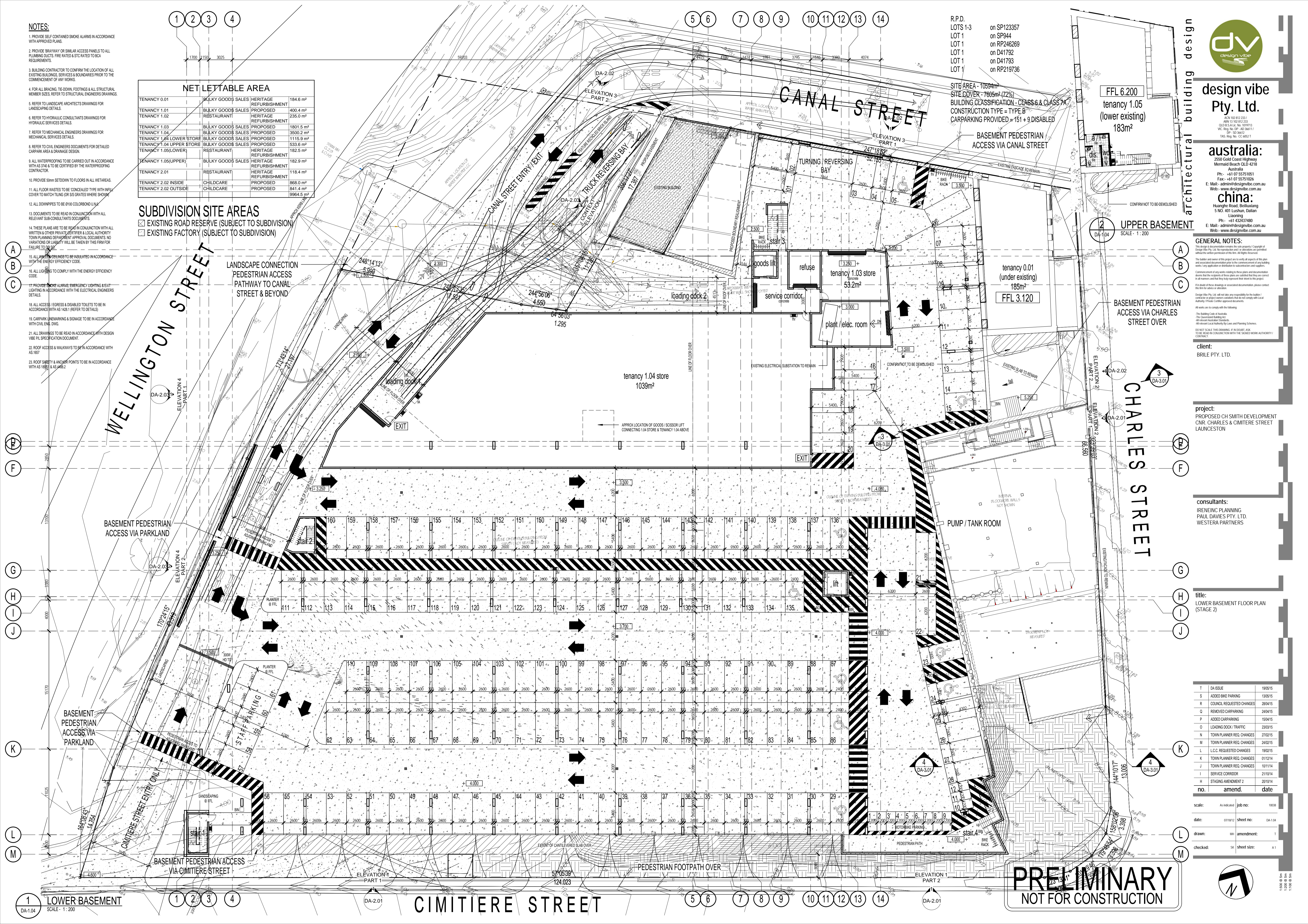
title:
 LOWER BASEMENT FLOOR PLAN
 (STAGE 2)

no.	amend.	date
T	DA ISSUE	19/05/15
S	ADDED BIKE PARKING	13/05/15
R	COUNCIL REQUESTED CHANGES	28/04/15
Q	REMOVED CARPARKING	24/04/15
P	ADDED CARPARKING	15/04/15
O	LOADING DOCK / TRAFFIC	23/03/15
N	TOWN PLANNER REQ. CHANGES	27/02/15
M	TOWN PLANNER REQ. CHANGES	24/02/15
L	L.C.C. REQUESTED CHANGES	19/02/15
K	TOWN PLANNER REQ. CHANGES	01/12/14
J	TOWN PLANNER REQ. CHANGES	10/11/14
I	SERVICE CORRIDOR	21/10/14
H	STAGING AMENDMENT 2	20/10/14

scale: As indicated job no: 10018
 date: 01/12/12 sheet no: DA-104
 drawn: 104 amendment: 1
 checked: SK sheet size: A1



PRELIMINARY
 NOT FOR CONSTRUCTION



NOTES:

1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. PROVIDE BRANNHY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS. FIRE RATED & STC RATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, TIE DOWN FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3740 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL WETAREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH INFL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN).
12. ALL DOWNPIPES TO BE Ø100 COLORBOND ULN/O.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUBCONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFICATES LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR DAMAGE TO DO SO.
15. ALL WALLS & CEILING TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1411 (REFER TO DETAILS).
19. CARPARK LINE MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG DWG.
21. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE PL SPECIFICATION DOCUMENT.
22. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1887.
23. ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1891.1 & AS 4882.

NET LETTABLE AREA			
TENANCY 0.01	BULKY GOODS SALES	HERITAGE REFURBISHMENT	184.6 m ²
TENANCY 1.01	BULKY GOODS SALES	PROPOSED	400.4 m ²
TENANCY 1.02	RESTAURANT	HERITAGE REFURBISHMENT	235.0 m ²
TENANCY 1.03	BULKY GOODS SALES	PROPOSED	1801.5 m ²
TENANCY 1.04	BULKY GOODS SALES	PROPOSED	3500.2 m ²
TENANCY 1.04 LOWER STORE	BULKY GOODS SALES	PROPOSED	1115.9 m ²
TENANCY 1.04 UPPER STORE	BULKY GOODS SALES	PROPOSED	532.6 m ²
TENANCY 1.05 (LOWER)	RESTAURANT	HERITAGE REFURBISHMENT	182.5 m ²
TENANCY 1.05 (UPPER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.9 m ²
TENANCY 2.01	RESTAURANT	HERITAGE REFURBISHMENT	118.4 m ²
TENANCY 2.02 INSIDE	CHILDCARE	PROPOSED	868.0 m ²
TENANCY 2.02 OUTSIDE	CHILDCARE	PROPOSED	841.4 m ²
			9964.5 m ²

- SUBDIVISION SITE AREAS**
- EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
 - EXISTING FACTORY (SUBJECT TO SUBDIVISION)

LANDSCAPE CONNECTION PEDESTRIAN ACCESS PATHWAY TO CANAL STREET & BEYOND

BASEMENT PEDESTRIAN ACCESS VIA PARKLAND

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CANAL STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

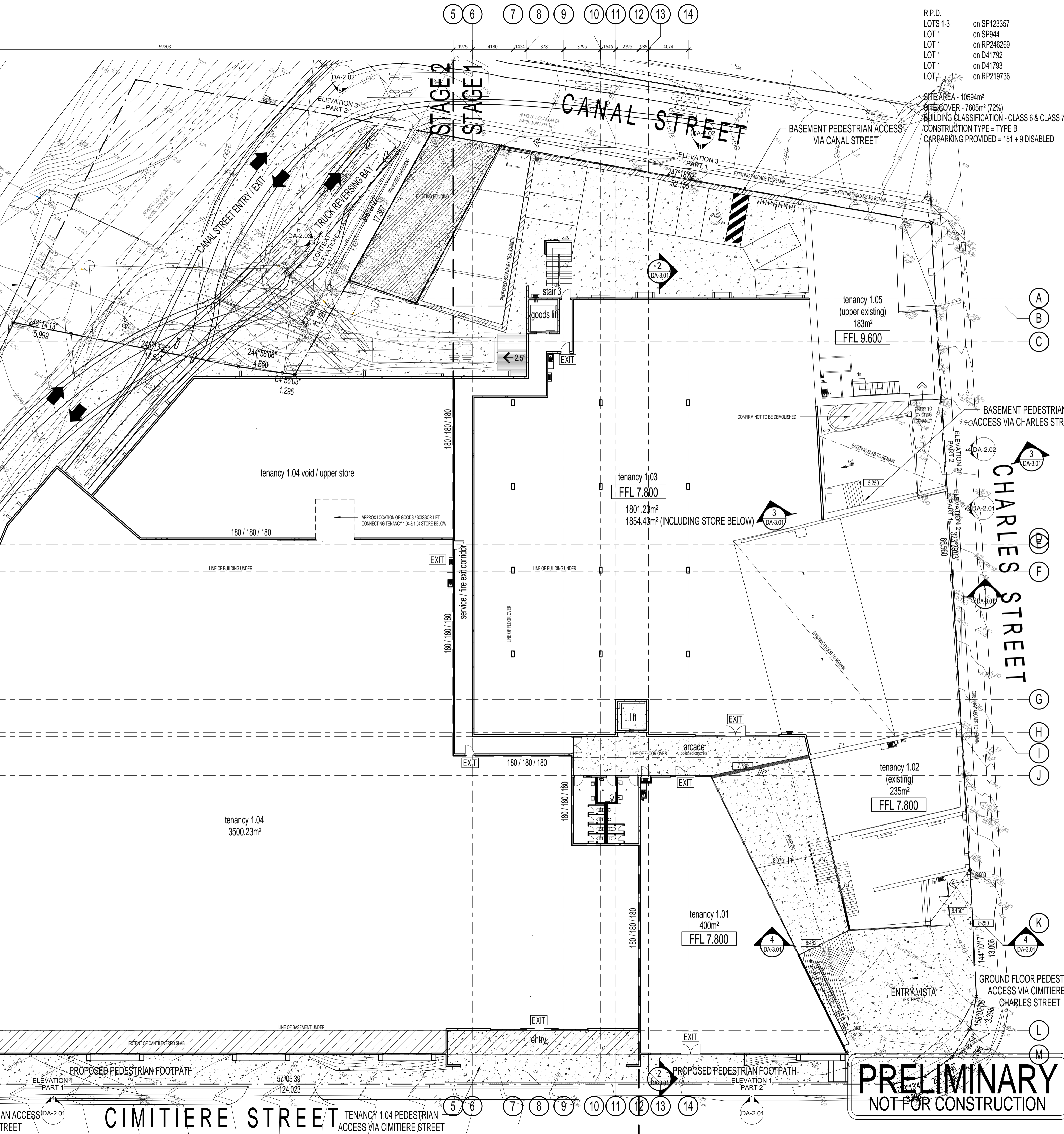
BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET

BASEMENT PEDESTRIAN ACCESS VIA CIMITIERE STREET



R.P.D. LOTS 1-3 on SP123357
 LOT 1 on SP944
 LOT 1 on RP246269
 LOT 1 on D41792
 LOT 1 on D41793
 LOT 1 on RP192736

SITE AREA - 10594m²
 SITE COVER - 7605m² (72%)
 BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
 CONSTRUCTION TYPE - TYPE B
 CARPARKING PROVIDED = 151 + 9 DISABLED

design vibe Pty. Ltd.

10/100 RIVERVIEW DRIVE
 MERMAID BEACH QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Beiliuxiang
 5 NO. 401 Lushun, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This stage 2 documentation remains the sole property of Copyright © Design Vibe Pty. Ltd. No reproduction or distribution is permitted without the written permission of this firm. All Rights Reserved.
 The liability and control of this project are solely the responsibility of this firm and associated documentation is the responsibility of the building contractor or project services provider. It is the responsibility of the building contractor or project services provider to ensure that they are compliant with all applicable laws and regulations.
 In the event of any change or associated documentation, please contact this firm for advice or alteration.
 Design Vibe Pty. Ltd. will not be responsible for the building contractor or project services provider. It is the responsibility of the building contractor or project services provider to ensure that they are compliant with all applicable laws and regulations.
 All works are to comply with the following:
 - The Building Code of Australia
 - The Queensland Building Act
 - All relevant Australian Standards
 - All relevant Local Authority By Laws and Planning Schemes.
 IN THE EVENT OF THIS DRAWING BEING REVISIONED, THE USER MUST BE KEEN TO READ IN CONJUNCTION WITH THE SEAIED WORK AUTHORITY CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

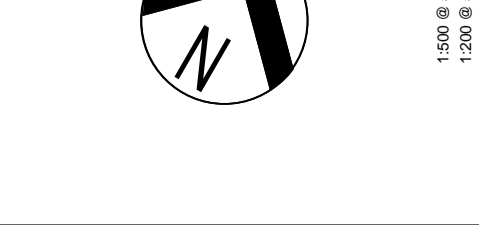
title:
 GROUND FLOOR PLAN (STAGE 2)

P	DA ISSUE	19/05/15
O	LOADING DOCK / TRAFFIC	23/03/15
N	TOWN PLANNER REQ. CHANGES	27/02/15
M	TOWN PLANNER REQ. CHANGES	24/02/15
L	L.C.C. REQUESTED CHANGES	19/02/15
K	TOWN PLANNER REQ. CHANGES	01/12/14
J	TOWN PLANNER REQ. CHANGES	10/11/14
I	SERVICE CORRIDOR	21/10/14
H	STAGING AMENDMENT 2	20/10/14
G	STAGING AMENDMENT	16/10/14
F	REQUESTED CHANGES	15/10/14
E	ADDED TENANCY 1.04	06/10/14
D	AMENDED CARPARK	15/08/14

no.	amend.	date
1		
2		
3		
4		

scale: As indicated job no: 10018
 date: 08/07/12 sheet no: DA-105
 drawn: 104 amendment: P
 checked: SK sheet size: A1

PRELIMINARY
 NOT FOR CONSTRUCTION

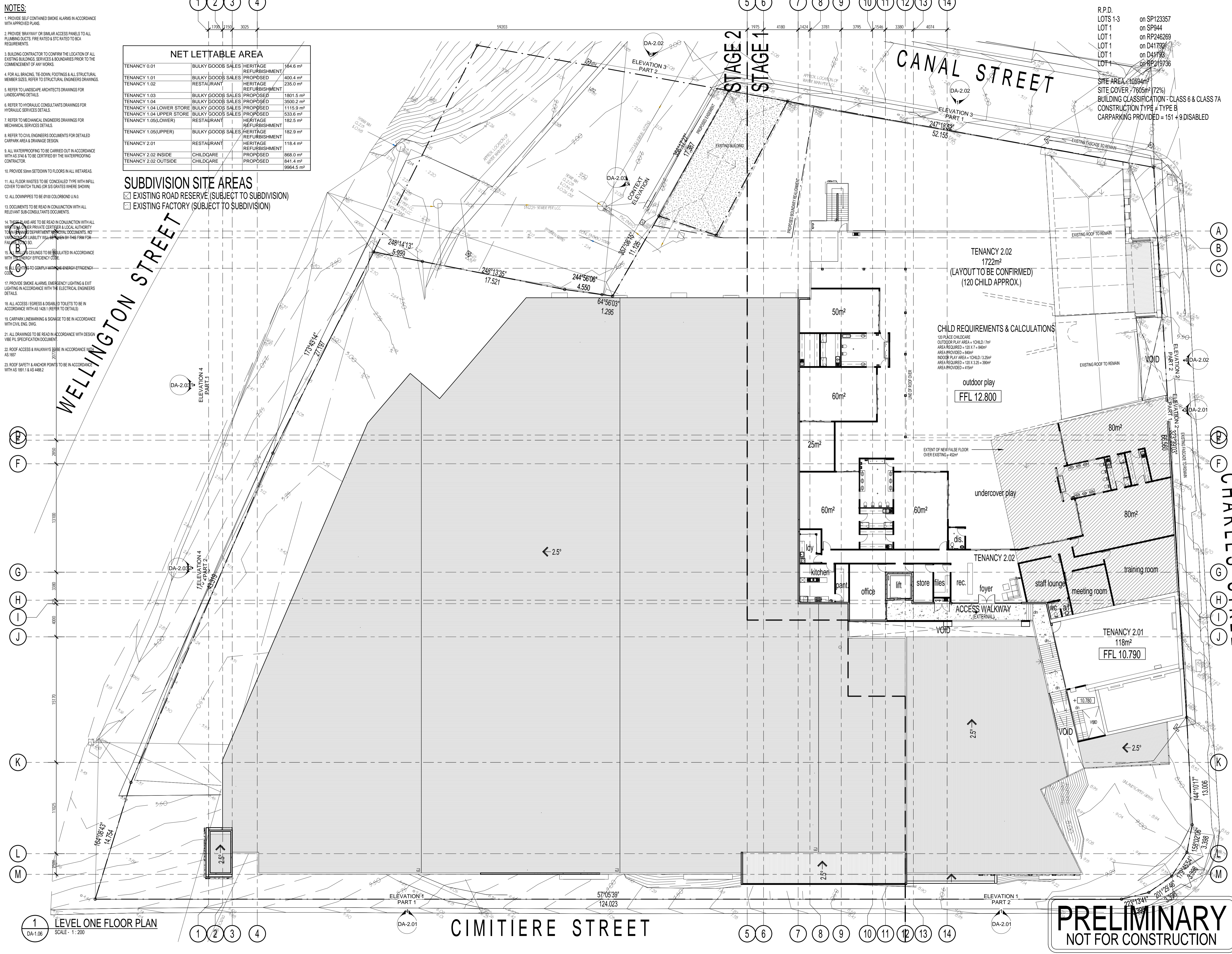


NOTES:

1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS. FIRE RATED & STC RATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & DIMENSIONS PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, TIE-DOWN, FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREAS & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3540 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL VETAREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH INFILL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN).
12. ALL DOWNPIPES TO BE Ø100 COLORBOND U.N.O.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB CONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN OWNER PRIVATE CERTIFICATES & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT TECHNICAL DOCUMENTS. NO VARIATION LIABILITY WILL BE ASSUMED BY THIS FIRM FOR FAILURE TO DO SO.
15. ALL CEILING LIGHTS TO BE RELATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL CEILING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
19. CARPARK LINES MARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
21. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE PL SPECIFICATION DOCUMENT.
22. ROOF ACCESS & WALKWAYS TO BE IN ACCORDANCE WITH AS 1657.
23. ROOF SAFETY & ANCHOR POINTS TO BE IN ACCORDANCE WITH AS 1891.1 & AS 4689.2.

NET LETTABLE AREA			
TENANCY 0.01	BULKY GOODS SALES	HERITAGE REFURBISHMENT	184.6 m ²
TENANCY 1.01	BULKY GOODS SALES	PROPOSED	400.4 m ²
TENANCY 1.02	RESTAURANT	HERITAGE REFURBISHMENT	235.0 m ²
TENANCY 1.03	BULKY GOODS SALES	PROPOSED	1801.5 m ²
TENANCY 1.04	BULKY GOODS SALES	PROPOSED	3500.2 m ²
TENANCY 1.04 LOWER STORE	BULKY GOODS SALES	PROPOSED	1115.9 m ²
TENANCY 1.04 UPPER STORE	BULKY GOODS SALES	PROPOSED	533.6 m ²
TENANCY 1.05(LOWER)	RESTAURANT	HERITAGE REFURBISHMENT	182.5 m ²
TENANCY 1.05(UPPER)	BULKY GOODS SALES	HERITAGE REFURBISHMENT	182.9 m ²
TENANCY 2.01	RESTAURANT	HERITAGE REFURBISHMENT	118.4 m ²
TENANCY 2.02 INSIDE	CHILD CARE	PROPOSED	868.0 m ²
TENANCY 2.02 OUTSIDE	CHILD CARE	PROPOSED	841.4 m ²
			9964.5 m ²

SUBDIVISION SITE AREAS
 EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
 EXISTING FACTORY (SUBJECT TO SUBDIVISION)



R.P.D.
 LOTS 1-3 on SP123357
 LOT 1 on SP944
 LOT 1 on RP246269
 LOT 1 on D41792
 LOT 1 on D41793
 LOT 1 on RP219736

SITE AREA = 10594m²
 SITE COVER = 7605m² (72%)
 BUILDING CLASSIFICATION - CLASS 6 & CLASS 7A
 CONSTRUCTION TYPE = TYPE B
 CARPARKING PROVIDED = 151 + 9 DISABLED

design vibe Pty. Ltd.
 architectural building design

australia:
 2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Beiliuxiang
 5 NO. 401 Lushan, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This design & documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction or disclosure is permitted without the written permission of this firm. All Rights Reserved.
 The liability and control of this project are solely the responsibility of the client and associated documentation prior to the commencement of any building works or any application or discharge to accommodate the client's project.
 Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they have approved the client's project.
 In the event of these drawings or associated documentation, please contact this firm for advice or alteration.
 Design Vibe Pty. Ltd. will not be responsible for the liability of any contractor or project partners unless they do not comply with Local Authority / Professional Engineer requirements.
 All works are to comply with the following:
 - The Building Code of Australia
 - The Queensland Building Act
 - All relevant Australian Standards
 - All relevant Local Authority By Laws and Planning Schemes.
 100% OF SCALE THIS DRAWING IS IN REDUCED SIZE.
 TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITIERE STREET
 LAUNCESTON

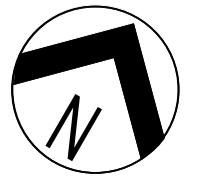
consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

title:
 LEVEL ONE FLOOR PLAN (STAGE 2)

no.	amend.	date
M	DA ISSUE	19/05/15
L	TOWN PLANNER REQ. CHANGES	27/02/15
K	TOWN PLANNER REQ. CHANGES	24/02/15
J	L.C.C. REQUESTED CHANGES	19/02/15
I	TOWN PLANNER REQ. CHANGES	01/12/14
H	TOWN PLANNER REQ. CHANGES	10/11/14
G	SERVICE CORRIDOR	21/10/14
F	STAGING AMENDMENT 2	20/10/14
E	STAGING AMENDMENT	16/10/14
D	REQUESTED CHANGES	15/10/14
C	ADDED TENANCY 1.04	06/10/14
B	TOWN PLANNER REQ. CHANGES	31/07/14
A	SKETCH ISSUE 1	25/07/14

scale: As indicated **job no:** 10018
date: 06/09/12 **sheet no:** DA-106
drawn: MJA **amendment:** M
checked: SK **sheet size:** A1

PRELIMINARY
 NOT FOR CONSTRUCTION



1:2000
 1:2000
 1:1000

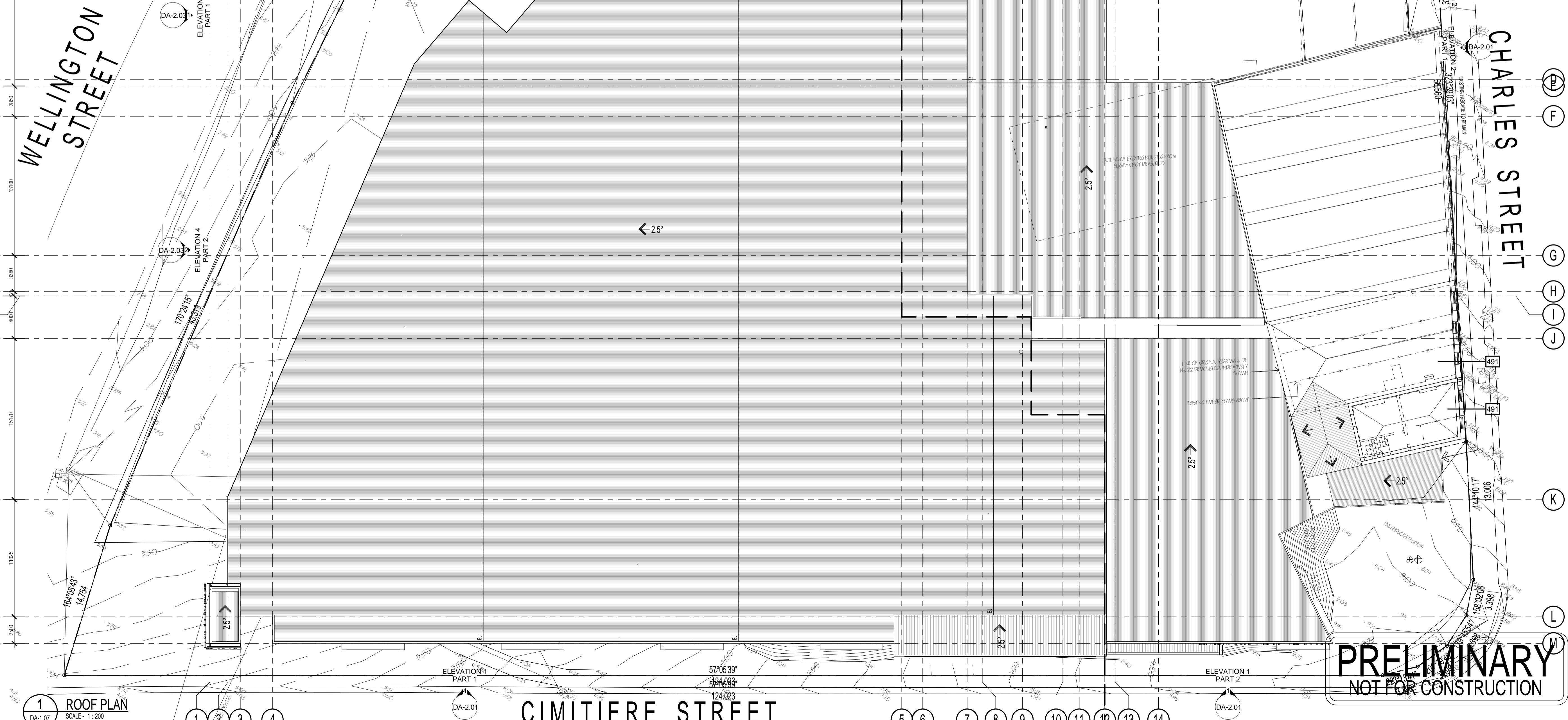
NOTES:

1. PROVIDE SELF CONTAINED SMOKE ALARMS IN ACCORDANCE WITH APPROVED PLANS.
2. PROVIDE BRAYWAY OR SIMILAR ACCESS PANELS TO ALL PLUMBING DUCTS, FIRE RATED & STIC RATED TO BCA REQUIREMENTS.
3. BUILDING CONTRACTOR TO CONFIRM THE LOCATION OF ALL EXISTING BUILDINGS, SERVICES & BOUNDARIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.
4. FOR ALL BRACING, TIE-DOWN FOOTINGS & ALL STRUCTURAL MEMBER SIZES, REFER TO STRUCTURAL ENGINEERS DRAWINGS.
5. REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPING DETAILS.
6. REFER TO HYDRAULIC CONSULTANTS DRAWINGS FOR HYDRAULIC SERVICES DETAILS.
7. REFER TO MECHANICAL ENGINEERS DRAWINGS FOR MECHANICAL SERVICES DETAILS.
8. REFER TO CIVIL ENGINEERS DOCUMENTS FOR DETAILED CARPARK AREA & DRAINAGE DESIGN.
9. ALL WATERPROOFING TO BE CARRIED OUT IN ACCORDANCE WITH AS 3541 & TO BE CERTIFIED BY THE WATERPROOFING CONTRACTOR.
10. PROVIDE 50mm SETDOWN TO FLOORS IN ALL VETAREAS.
11. ALL FLOOR WASTES TO BE CONCEALED TYPE WITH HULL COVER TO MATCH TILING (OR SS GRATES WHERE SHOWN).
12. ALL DOWNPIPES TO BE Ø100 COLORBOND U.N.I.
13. DOCUMENTS TO BE READ IN CONJUNCTION WITH ALL RELEVANT SUB-CONSULTANTS DOCUMENTS.
14. THESE PLANS ARE TO BE READ IN CONJUNCTION WITH ALL WRITTEN & OTHER PRIVATE CERTIFIER & LOCAL AUTHORITY TOWN PLANNING DEPARTMENT APPROVAL DOCUMENTS. NO VARIATIONS OR LIABILITY WILL BE TAKEN BY THIS FIRM FOR WORK NOT TO DO SO.
15. ALL WALLS & CEILINGS TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE.
16. ALL LIGHTING TO COMPLY WITH THE ENERGY EFFICIENCY CODE.
17. PROVIDE SMOKE ALARMS, EMERGENCY LIGHTING & EXIT LIGHTING IN ACCORDANCE WITH THE ELECTRICAL ENGINEERS DETAILS.
18. ALL ACCESS / EGRESS & DISABLED TOILETS TO BE IN ACCORDANCE WITH AS 1428.1 (REFER TO DETAILS).
19. CARPARK LINEMARKING & SIGNAGE TO BE IN ACCORDANCE WITH CIVIL ENG. DWG.
21. ALL DRAWINGS TO BE READ IN ACCORDANCE WITH DESIGN VIBE PL SPECIFICATION DOCUMENT.

NOTES	
020	COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC. TYP.)
178	FEATURE 'HEBEL' OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
485	INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
486	EXISTING OPENINGS INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
487	EXISTING WALL TO REMAIN
490	EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
491	EXISTING ROOF TO BE REINSTATED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
497	FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
501	FEATURE PLANTED GREEN WALL, ELMICH VGM OR SIMILAR TO MANUFACTURERS SPECIFICATION
503	SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
504	FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
507	NEW WALL TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
511	ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
512	FEATURE GABION MATTRESS.
513	EPS SANDWICH WALL PANELS.
515	HERITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (& THE PERMIT) FOR PROJECT NO 7063 EXH1A TO EXH1A INCLUSIVE, HDA01B TO HDA04B INCLUSIVE, & HDA05B TO HDA09B, AND THE HERITAGE IMPACT STATEMENT DATED JUNE 2007 BY PAUL DAVIES PTY LTD.
516	TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERITAGE FACADE.

SUBDIVISION SITE AREAS

- EXISTING ROAD RESERVE (SUBJECT TO SUBDIVISION)
- EXISTING FACTORY (SUBJECT TO SUBDIVISION)



PRELIMINARY
NOT FOR CONSTRUCTION

1 ROOF PLAN
DA-1.07 SCALE: 1:200

architectural building design

design vibe
Pty. Ltd.

ACN 102 812 231 / ABN 13 102 812 231
C/O 5/4 VIC. ST. MELB. VIC. 3000
VIC. Reg. No. EP AD 36611 / GST Reg. No. 152 86427
TAS. Reg. No. CC 66627

australia:

2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:

Huanghe Road, Beiluxiang
5 No. 401 Lushun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:

This design & documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction or distribution is permitted without the written permission of this firm. All Rights Reserved.

The liability and/or extent of the project are limited to the extent of the plan and associated documentation prior to the commencement of any building works or application or submission to government and/or regulators.

Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they have approved the plan to the project.

In the event of these drawings or associated documentation, please contact this firm for advice or alteration.

Design Vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless they do not comply with Local Authority (Public) or other approved documents.

All works are to comply with the following:

- The Building Code of Australia.
- The Queensland Building Act.
- All relevant Australian Standards.
- All relevant Local Authority By-Laws and Planning Schemes.

client:

BRILE PTY. LTD.

project:

PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITIERE STREET
LAUNCESTON

consultants:

IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

title:

ROOF PLAN (STAGE 2)

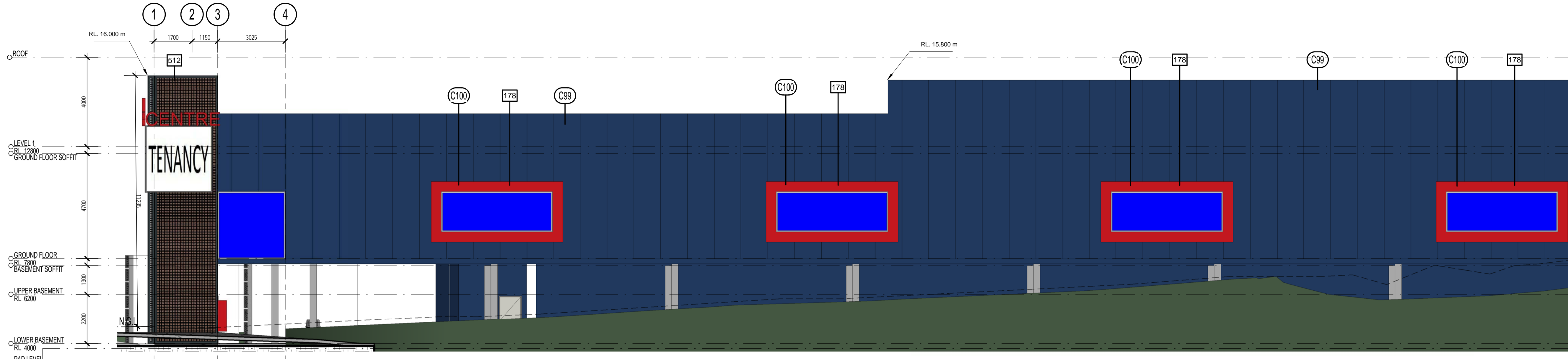
no.	amend.	date
H	DA ISSUE	19/05/15
G	TOWN PLANNER REQ. CHANGES	24/02/15
F	L.C.C. REQUESTED CHANGES	19/02/15
E	TOWN PLANNER REQ. CHANGES	01/12/14
D	TOWN PLANNER REQ. CHANGES	10/11/14
C	STAGING AMENDMENT 2	20/10/14
B	STAGING AMENDMENT	16/10/14
A	SKETCH ISSUE 1	15/08/14

scale: As indicated job no: 10018

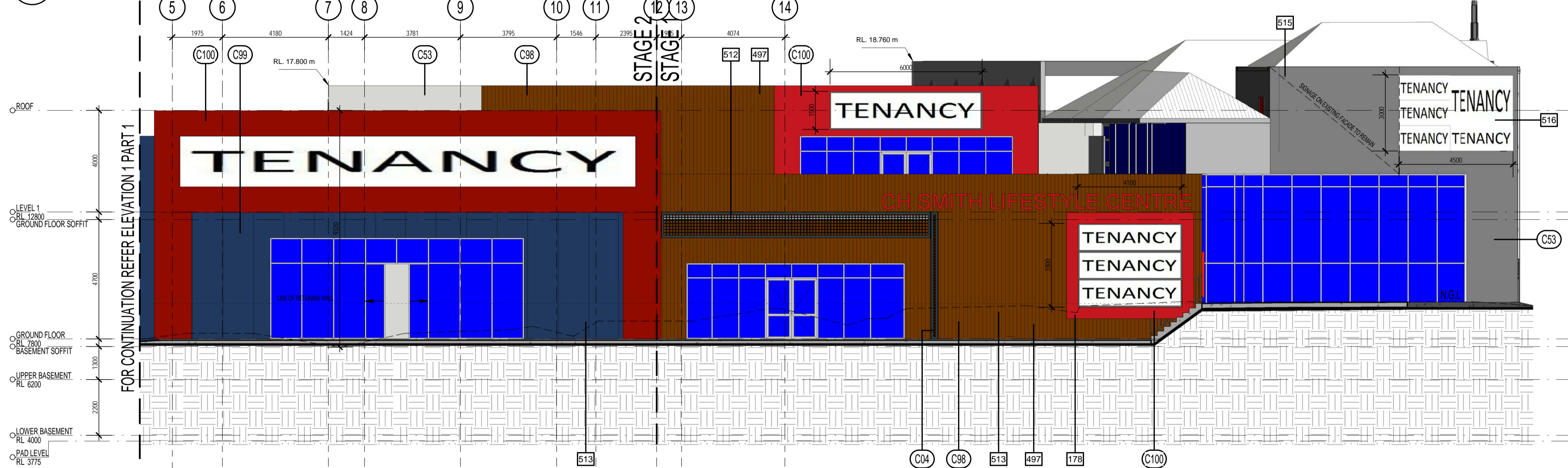
date: 09/10/12 sheet no: DA-1.07

drawn: 101 amendment: H

checked: SK sheet size: A1



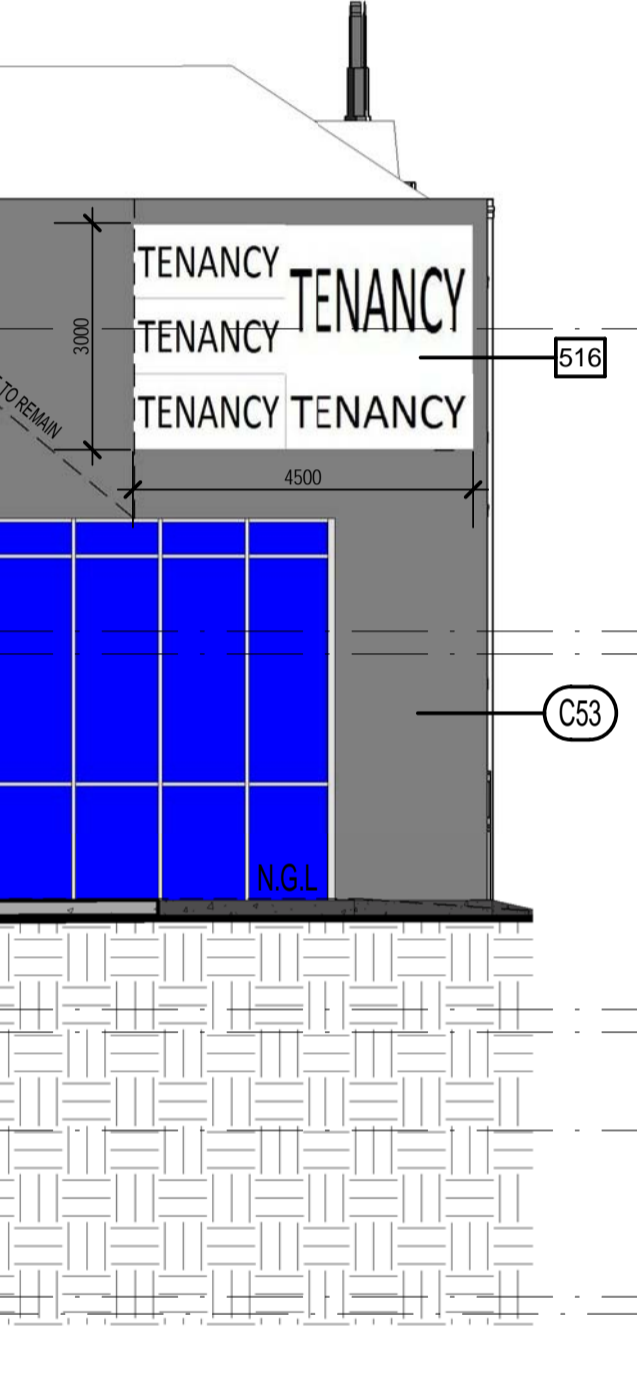
4 ELEVATION 1 PART 1
DA-2.01 SCALE: 1:100



1 ELEVATION 1 PART 2
DA-2.01 SCALE: 1:100



3 ELEVATION 2 PART 1
DA-2.01 SCALE: 1:100



2 CIMITIERE STREET EXISTING FACADE
DA-2.01 SCALE - NTS



NOTES	
020	COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC TYP.)
178	FEATURE 'HEBEL' OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
485	INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
486	EXISTING OPENING INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
487	EXISTING WALL TO REMAIN
490	EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
491	EXISTING ROOF TO BE REINSTATED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
497	FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
501	FEATURE PLANTED GREEN WALL 'ELMIICH VGM' OR SIMILAR TO MANUFACTURERS SPECIFICATION
503	SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
504	FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
507	NEW WALL TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
511	ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
512	FEATURE GABION MATRESS
513	EPS SANDWICH WALL PANELS
515	HERITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (& THC PERMIT), FOR PROJECT NO 7-063-EX01A TO EX07A INCLUSIVE. HEAD01B TO HEAD04B INCLUSIVE, & HEAD05 TO HEAD08; AND THE HERITAGE IMPACT STATEMENT DATED JUNE 2007, BY PAUL DAVIES PTY LTD.
516	TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERITAGE FACADE

COLOUR LEGEND	
C04	RESENE 'MELTECA CHARCOAL GREY'
C07	RESENE 'AMBER GREY'
C53	RESENE 'WHITE'
C97	FEATURE STONE FINISH TO MATCH EXISTING HERITAGE FEATURES
C98	WESTERN RED CEDAR
C99	DULUX 'BLUE LOBELIA'
C100	SOLVER 'SIGNAL RED'
C101	RESENE 'PARIS DAISY'

architectural building design

design vibe Pty. Ltd.
ACN 102 812 231 / ABN 13 102 812 231
C/O B.S.U.C. No. 9000713
VIC. Reg. No. DP AD 36611 / IP 52 36612
TAS. Reg. No. CC 6852 1

australia:
2550 Gold Coast Highway
Mermaid Beach QLD 4218
Australia
Ph: +61 07 55751051
Fax: +61 07 55751026
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

china:
Huanghe Road, Belluxiang
5 NO. 401 Lushun, Dalian
Liaoning
Ph: +61 432437480
E-Mail: admin@designvibe.com.au
Web: www.designvibe.com.au

GENERAL NOTES:
This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction and/or distribution are permitted without the written permission of this firm. All Rights Reserved.
The liability and/or responsibility of this project are solely of Design Vibe Pty. Ltd. and associated documentation is the commitment of any building contractor, any application or distribution to subcontractors and suppliers.
Commencement of any works relating to these plans and documentation deems that the recipient of these plans are advised that they are correct in all respects and that they hold responsible for their use on the project.
If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
Design Vibe Pty. Ltd. will not be held responsible for the liability of contractor or project services unless that do not comply with Local Authority/Professional Engineer approved documents.
All works are to comply with the following:
- The Building Code of Australia
- The Queensland Building Act
- All relevant Australian Standards
- All relevant Local Authority By Laws and Planning Schemes.
SHEET SCALE: THIS DRAWING IS IN SEVERAL JOBS.
TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
BRILE PTY. LTD.

project:
PROPOSED CH SMITH DEVELOPMENT
CNR. CHARLES & CIMITERE STREET
LAUNCESTON

consultants:
IRENEIC PLANNING
PAUL DAVIES PTY. LTD.
WESTERA PARTNERS

title:
ELEVATION SHEET 1 (STAGE 2)

no.	amend.	date
J	DA ISSUE	19/05/15
I	TOWN PLANNER REQ. CHANGES	27/02/15
H	TOWN PLANNER REQ. CHANGES	24/02/15
G	L.C.C. REQUESTED CHANGES	19/02/15
F	TOWN PLANNER REQ. CHANGES	01/12/14
E	TOWN PLANNER REQ. CHANGES	10/11/14
D	STAGING AMENDMENT 2	20/10/14
C	STAGING AMENDMENT	16/10/14
B	TOWN PLANNER REQ. CHANGES	31/07/14
A	SKETCH ISSUE 1	25/07/14

scale: 1:100 job no: 10018
date: 09/12/12 sheet no: DA-2.01
drawn: 104 amendment: J
checked: SK sheet size: A1

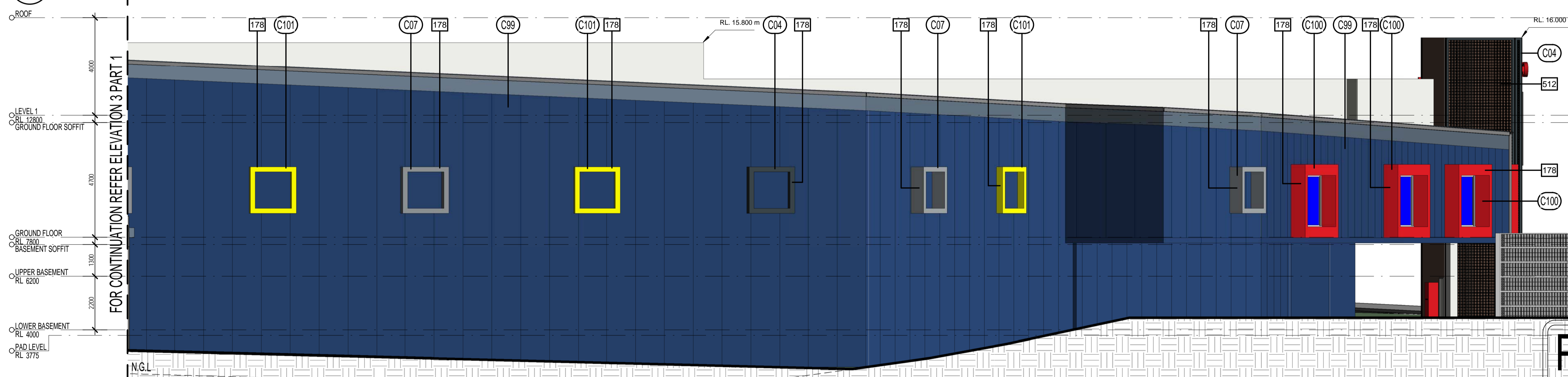
PRELIMINARY
NOT FOR CONSTRUCTION



4 ELEVATION 2 PART 2
DA-2.02 SCALE: 1:100



1 ELEVATION 3 PART 1
DA-2.02 SCALE: 1:100



5 ELEVATION 3 PART 2
DA-2.02 SCALE: 1:100

NOTES

- 020 COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC. TYP.)
- 178 FEATURE HEBEL OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
- 485 INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 486 EXISTING OPENING INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 487 EXISTING WALL TO REMAIN
- 490 EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 491 EXISTING ROOF TO BE REINSTATED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 497 FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
- 501 FEATURE PLANTED GREEN WALL 'ELMICH YGM' OR SIMILAR TO MANUFACTURERS SPECIFICATION
- 503 SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
- 504 FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
- 507 NEW WALL TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 511 ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
- 512 FEATURE GABION MATTRESS.
- 513 EPS SANDWICH WALL PANELS.
- 515 HERITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (S/THC PERMIT). FOR PROJECT NO 7-063:EX01A TO EX07A INCLUSIVE, HDA01B TO HDA04B INCLUSIVE, & HDA05B TO HDA09B; AND THE HERITAGE IMPACT STATEMENT DATED JUNE 2007, BY PAUL DAVIES PTY LTD.
- 516 TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERITAGE FACADE.

COLOUR LEGEND

C04	RESENE 'MELTECA CHARCOAL GREY'
C07	RESENE 'AMBER GREY'
C53	RESENE 'WHITE'
C97	FEATURE STONE FINISH TO MATCH EXISTING HERITAGE FEATURES
C98	WESTERN RED CEDAR
C99	DULUX 'BLUE LOBELIA'
C100	SOLVER 'SIGNAL RED'
C101	RESENE 'PARIS DAISY'

STAGE 1
STAGE 2

FOR CONTINUATION REFER ELEVATION 3 PART 2

**PRELIMINARY
NOT FOR CONSTRUCTION**

architectural building design

design vibe Pty. Ltd.
 ACN 102 812 231 / ABN 13 102 812 231
 CRICOS No. 52379 (0001)
 VIC. Reg. No. 'EP' AD 36611 /
 QWS Reg. No. 1236472
 TAS. Reg. No. CC 66527

australia:
 2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Belluxiang
 5 NO. 401 Lushun, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This design documentation remains the sole property of Copyright © design vibe Pty. Ltd. No reproduction or distribution is permitted without the written permission of this firm. All Rights Reserved.
 The reader and owner of this project are advised that the design and associated documentation is for the convenience of any building contractor or project services and that they are not to be used for any other purpose or to be used in any other project.
 If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
 design vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless they do not comply with local authority / public safety approved documents.
 All works are to comply with the following:
 - The Building Code of Australia.
 - The Queensland Building Act.
 - All relevant Australian Standards.
 - All relevant Local Authority By Laws and Planning Schemes.
 DESIGN SCALE: THIS DRAWING IS IN METRIC UNITS.
 TO BE READ IN CONJUNCTION WITH THE SCALED WORK AUTHORITY CONTRACT.

client:
 BRILE PTY. LTD.

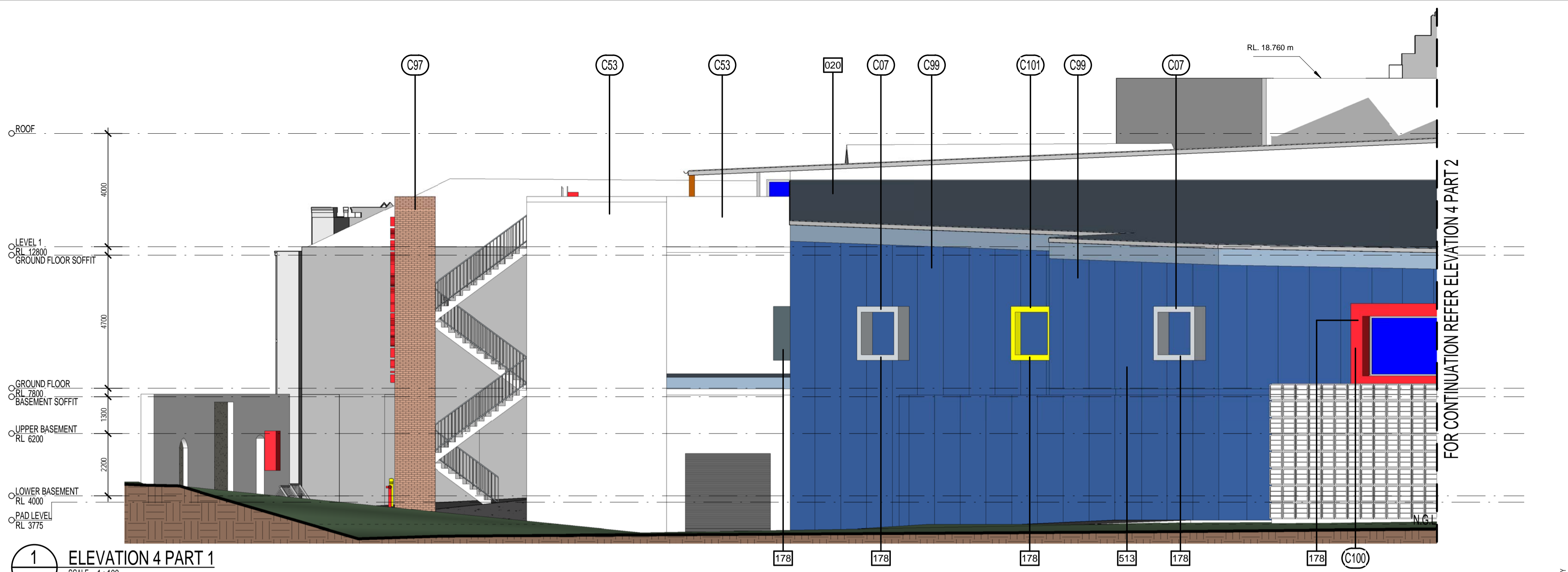
project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITERE STREET
 LAUNCESTON

consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

no.	amend.	date
J	DA ISSUE	19/05/15
I	TOWN PLANNER REQ. CHANGES	27/02/15
H	TOWN PLANNER REQ. CHANGES	24/02/15
G	L.C.C. REQUESTED CHANGES	19/02/15
F	TOWN PLANNER REQ. CHANGES	01/12/14
E	TOWN PLANNER REQ. CHANGES	10/11/14
D	STAGING AMENDMENT 2	20/10/14
C	STAGING AMENDMENT	16/10/14
B	TOWN PLANNER REQ. CHANGES	31/07/14
A	SKETCH ISSUE 1	25/07/14

scale: 1:100 job no: 10018
 date: 09/12/12 sheet no: DA-2.02
 drawn: 104 amendment: J
 checked: SK sheet size: A1

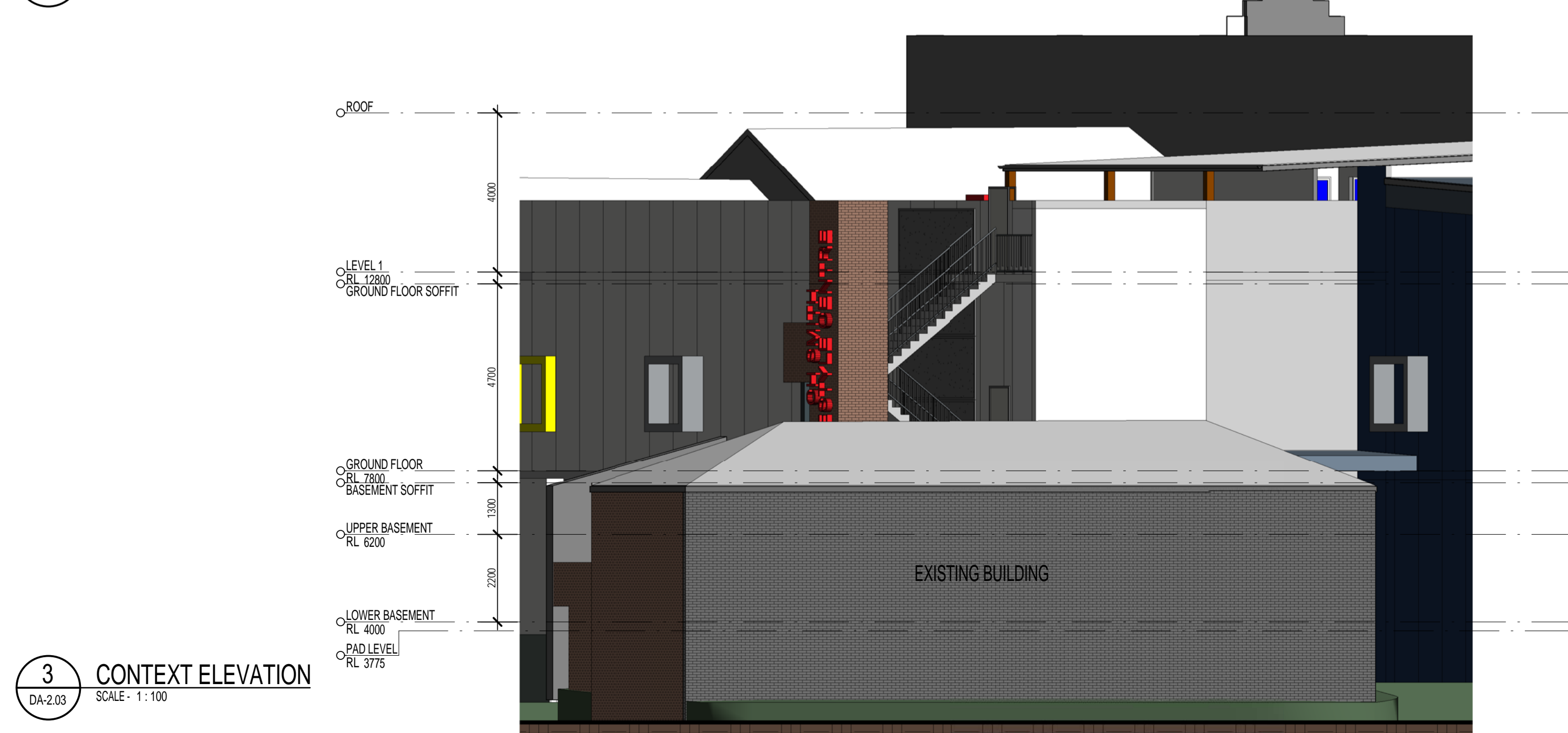
1:000 0:500 1:200 0:500 1:100 0:500



1 ELEVATION 4 PART 1
DA-2.03 SCALE: 1:100



2 ELEVATION 4 PART 2
DA-2.03 SCALE: 1:100



3 CONTEXT ELEVATION
DA-2.03 SCALE: 1:100

NOTES	
020	COLORBOND MONOCLAD ROOF SHEETING (PROVIDE EXPANSION JOINTS IN ROOF SHEETING WHERE REQUIRED IN ACCORDANCE WITH MANUFACTURERS SPEC.TYP.)
178	FEATURE HEBEL OR EXPANDED POLYSTYRENE MOULDED AWNING FIXED TO STRUCTURE & FINISHED IN ACCORDANCE WITH MANUF. REQUIREMENTS (TYP.)
485	INFILL EXISTING OPENINGS TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
486	EXISTING OPENING INFILLED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
487	EXISTING WALL TO REMAIN
490	EXISTING FLOOR TO REMAIN TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
491	EXISTING ROOF TO BE REINSTATED TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
497	FEATURE RECYCLED TIMBER CLADDING TO EXTERIOR OF WALL - SEL. PAINT FINISH AS PER SCHEDULE (TYPICAL)
501	FEATURE PLANTED GREEN WALL ELMICH VGM OR SIMILAR TO MANUFACTURERS SPECIFICATION
503	SELECTED TENANCY SIGNAGE PANELS TO MANUFACTURERS DETAIL
504	FEATURE 3D TEXT TO SIGNWRITER DETAIL (APPROVAL REQUIRED)
507	NEW WALL TO HERRITAGE CONSULTANTS DETAILS (TYPICAL)
511	ALL NEW WINDOWS & DOORS IN EXISTING FACADE TO HERITAGE CONSULTANTS DETAILS (TYPICAL)
512	FEATURE GABION MATTRESS.
513	EPS SANDWICH WALL PANELS.
515	HERRITAGE FACADES & FABRIC IN ACCORDANCE WITH THE DRAWINGS BY PAUL DAVIES PTY LTD (8 THE PERMIT), FOR PROJECT NO 7-863EX1A TO EX07A INCLUSIVE, HDA01B TO HDA04B INCLUSIVE, & HDA05B TO HDA08B; AND THE HERRITAGE IMPACT STATEMENT DATED JUNE 2007, BY PAUL DAVIES PTY LTD.
516	TENANCY SIGNS TO BE PAINTED TO REFLECT EXISTING HERRITAGE FACADE.

COLOUR LEGEND	
C04	RESENE MELTECA CHARCOAL GREY
C07	RESENE AMBER GREY
C53	RESENE WHITE
C97	FEATURE STONE FINISH TO MATCH EXISTING HERITAGE FEATURES
C98	WESTERN RED CEDAR
C99	DULUX BLUE LOBELIA
C100	SOLVER SIGNAL RED
C101	RESENE PARIS DAISY

architectural building design

design vibe Pty. Ltd.
 ACN 102 812 231 / ABN 13 102 812 231
 CRICOS No. 604 190723
 VC Reg No. 08 AD 36611 / 09 52 8642
 TAS Reg No. CC 6622 7

australia:
 2550 Gold Coast Highway
 Mermaid Beach QLD 4218
 Australia
 Ph: +61 07 55751051
 Fax: +61 07 55751026
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

china:
 Huanghe Road, Belluxiang
 5 NO. 401 Lushun, Dalian
 Liaoning
 Ph: +61 432437480
 E-Mail: admin@designvibe.com.au
 Web: www.designvibe.com.au

GENERAL NOTES:
 This design documentation remains the sole property of Copyright of Design Vibe Pty. Ltd. No reproduction and/or disclosure are permitted without the written permission of this firm. All Rights Reserved.
 The liability and control of this project are solely of Design Vibe Pty. Ltd. and associated documentation prior to the commencement of any building works. Any application or disclosure to government and regulators. Commencement of any works relating to these plans and documentation deems that the recipient of these plans are satisfied that they are correct in all respects and that they represent the intent of the project.
 If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
 Design Vibe Pty. Ltd. will not be responsible for the liability of contractor or project services unless they do not comply with local authority (Council) approved documents.
 All works are to comply with the following:
 - The Building Code of Australia
 - The Queensland Building Act
 - All relevant Australian Standards
 - All relevant Local Authority By Laws and Planning Schemes.
 DESIGN SCALE: THIS DRAWING, IF IN SEVERAL JOBS, IS TO BE IN CONFORMANCE WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITERE STREET
 LAUNCESTON

consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

title:
 ELEVATION SHEET 3 (STAGE 2)

no.	amend.	date
F	DA ISSUE	19/05/15
E	TOWN PLANNER REQ. CHANGES	27/02/15
D	L.C.C. REQUESTED CHANGES	19/02/15
C	TOWN PLANNER REQ. CHANGES	01/12/14
B	TOWN PLANNER REQ. CHANGES	10/11/14
A	STAGING AMENDMENT 2	20/10/14

scale: 1:100 job no: 10018
 date: 10/10/14 sheet no: DA-2.03
 drawn: 101 amendment: F
 checked: SK sheet size: A1

PRELIMINARY
 NOT FOR CONSTRUCTION



INTERNAL LININGS
 CEILINGS - SQUARE SET ALL PLASTERBOARD WALL / CEILING JUNCTIONS (TYP.)
 REFER TO MECHANICAL ENGINEERS DRAWINGS FOR LOCATION OF ACCESS PANELS & AIR FLOWING MECHANICAL VENTILATION REGISTERS.

13mm PLASTERBOARD LINING TO INSULATE US OF FLOOR TRUSSES / CEILING FRAMING
 - SEL. PAINT FINISH (TYP.)

WALLS - 10mm PLASTERBOARD LINING TO INSULATE (VILLABOARD TO WEATHERS) - SEL. 2500mm x 1200mm x 12mm GIB GYPSUM BOARD TO WEATHERS
 PACK OUT WALL LINING FROM CONC. TILT-UP FORMWORK TO ACCOMMODATE PLUMBING WHERE REQUIRED
 AREAS (TYP.)

ALL WALLS & CEILING TO BE INSULATED IN ACCORDANCE WITH THE ENERGY EFFICIENCY CODE
 DOORS - ALL INTERNAL DOORS TO BE AS PER CODE OF PRACTICE FOR ACCESSIBLE BUILDINGS
 DESIGN VIBE AUSTRALIA
 Mordialloc VIC 3213
 PH: +61 3 9593 2333
 FAX: +61 3 9593 2333
 WWW: www.designvibe.com.au
 DESIGN VIBE CHINA
 Huanghe Road, Beiluxiang
 Dalian Liaoning
 PH: +61 432437480
 WWW: www.designvibe.com.au

GENERAL NOTES:
 This design & documentation remains the sole property of Copyright © Design Vibe Pty. Ltd. No reproduction or use is permitted without the written permission of this firm. All Rights Reserved.
 The liability and control of this project are solely the responsibility of the client and associated documentation prior to the commencement of any building works. Any application or distribution to subcontractors and suppliers.
 Commencement of any works relating to these plans and documentation denotes that the recipient of these plans are advised that they are correct at all times and that they hold responsible for their use.
 If in doubt of these drawings or associated documentation, please contact this firm for advice or alteration.
 Design Vibe Pty. Ltd. will not take any responsibility for the liability of contractor or project services unless they do not comply with Local Authority / Council approved documentation.
 All works are to comply with the following:
 The Building Code of Australia
 The Queensland Building Act
 All relevant Australian Standards
 All relevant Local Authority By Laws and Planning Schemes.
 DESIGN SCALE: THIS DRAWING, IF IN REDUCED SIZE, TO BE READ IN CONJUNCTION WITH THE SIGNED WORK AUTHORITY / CONTRACT.

client:
 BRILE PTY. LTD.

project:
 PROPOSED CH SMITH DEVELOPMENT
 CNR. CHARLES & CIMITERE STREET
 LAUNCESTON

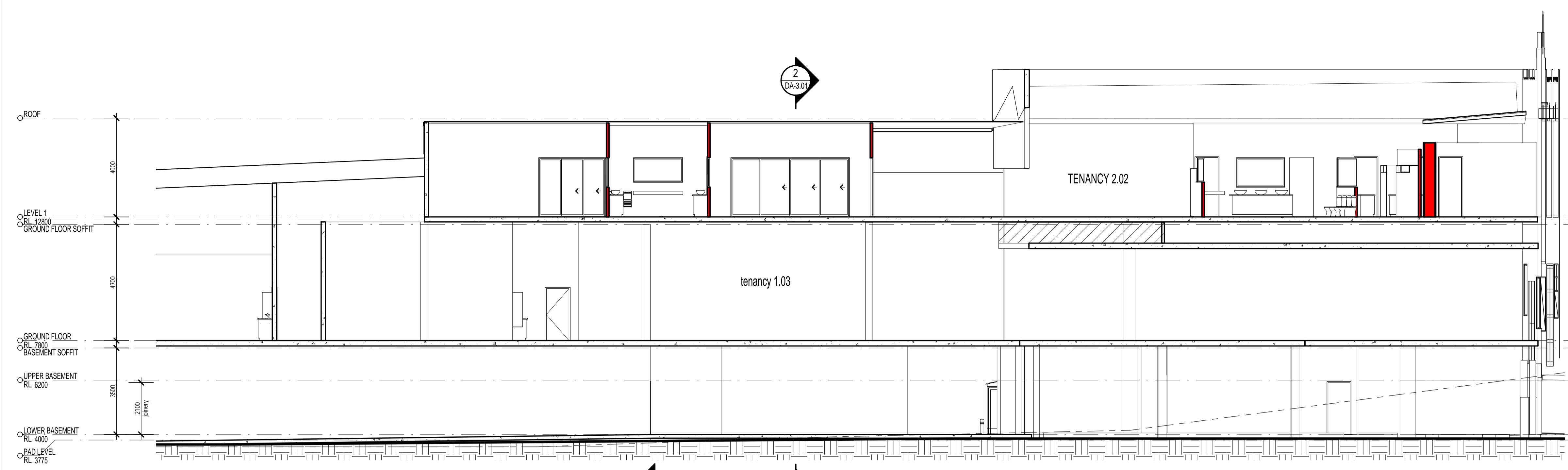
consultants:
 IRENEIC PLANNING
 PAUL DAVIES PTY. LTD.
 WESTERA PARTNERS

title:
 SECTIONS (STAGE 2)

no.	amend.	date
D	DA ISSUE	19/05/15
C	ADDED PEDESTRIAN SECTIONS	13/05/15
B	L.C.C. REQUESTED CHANGES	19/02/15
A	SKETCH ISSUE 1	22/08/14

scale:	1:100	job no:	10018
date:	09/10/12	sheet no:	DA-3.01
drawn:	SH	amendment:	D
checked:	SK	sheet size:	A1

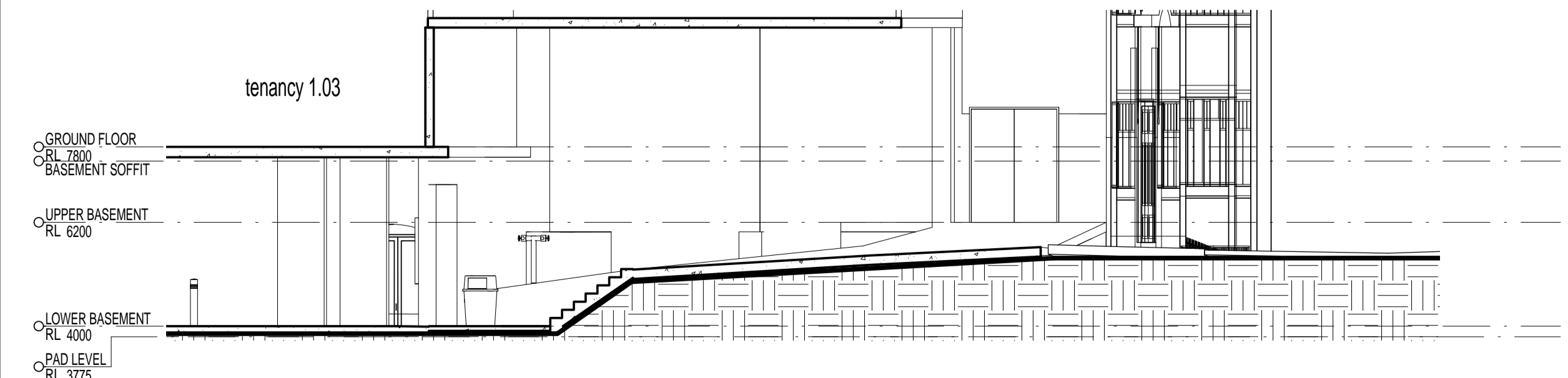
PRELIMINARY
 NOT FOR CONSTRUCTION



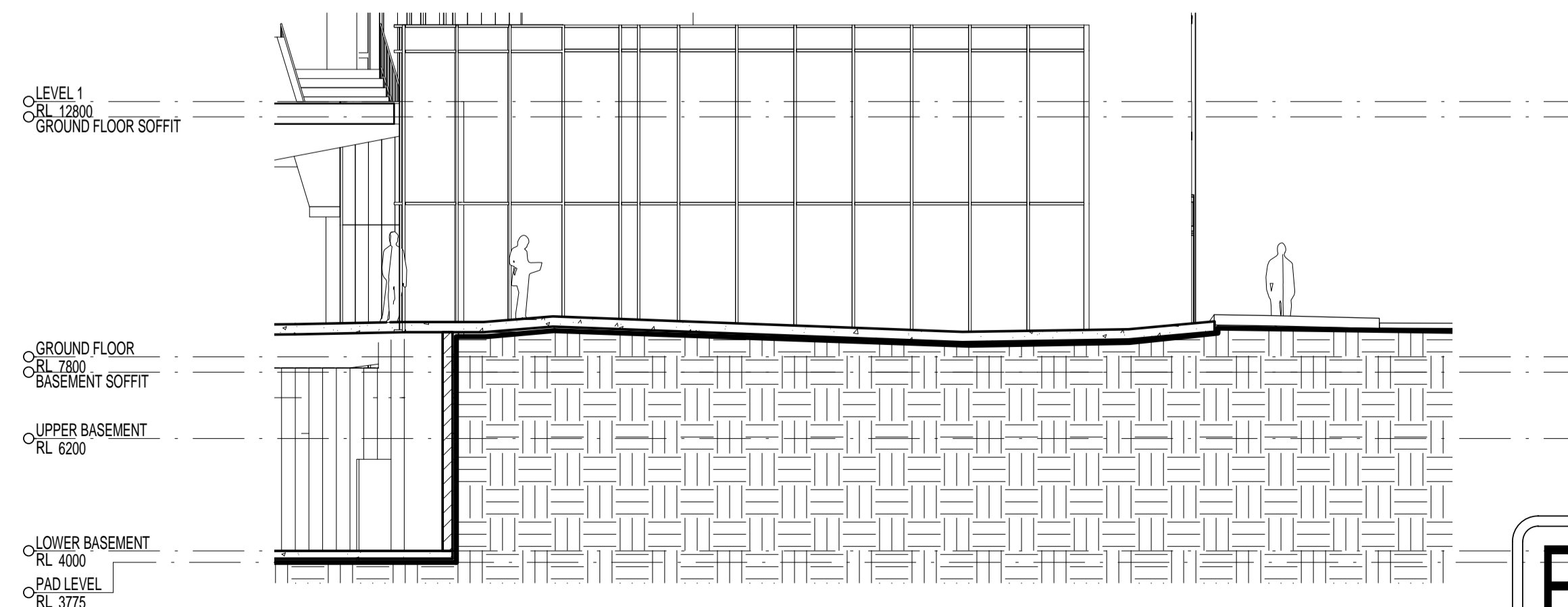
1 SECTION 1
 DA-3.01 SCALE - 1:100



2 SECTION 2
 DA-3.01 SCALE - 1:100



3 CHARLES STREET PEDESTRIAN ACCESS
 DA-3.01 SCALE - 1:100



4 CORNER OF CHARLES & CIMITERO STREET PEDESTRIAN ACCESS
 DA-3.01 SCALE - 1:100

Postal Address

PO Box 63
 Riverside 7250
 W. Brisbane
 E. admin@city.com.au
 T: 07 550 0000
 F: 07 550 0001
 P: 07 550 0002
 C: 07 550 0003

City Pty Ltd

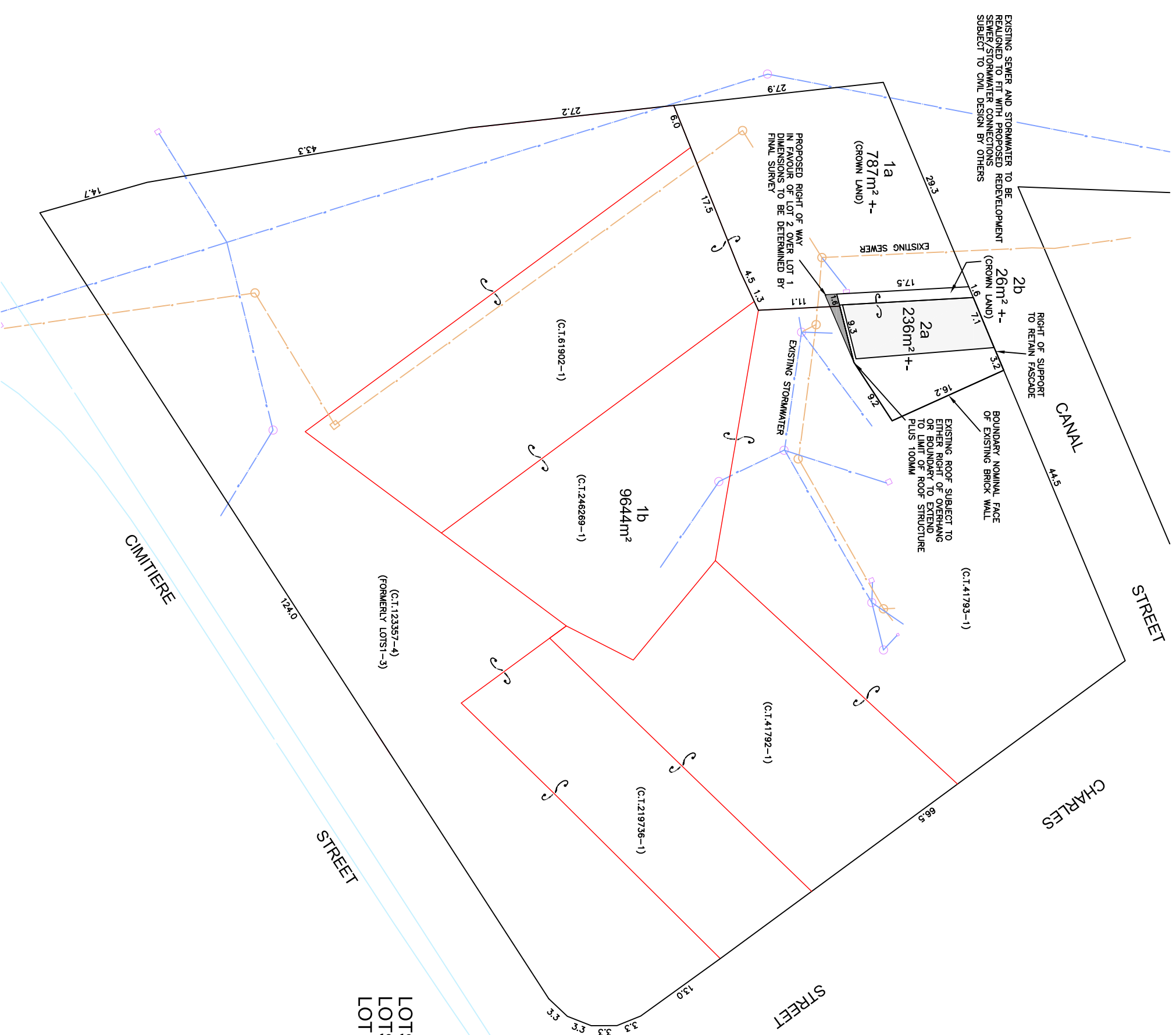
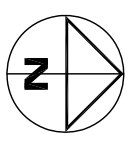
ABN 27 014 609 900
 Architectural
 ABP No. CC47747
 Structural / Civil
 ABP No. CC16331



PRELIMINARY - NOT FOR CONSTRUCTION

ISSUE	DATE	ISSUED FOR	REV.
01	28.11.2014	INFORMATION	A
02	05.02.2015	COUNCIL APPROVAL	A

LOTS 2a & 2b TO BE ADDED TO FORM LOT2 262m² +-
 LOTS 1a & 1b TO BE ADDED TO FORM LOT 1 10431m² +-
 LOT 1b BEING THE CONSOLIDATION OF EXISTING TITLES



DIMENSIONS ARE IN METRES. DO NOT SCALE. CHECK AND VERIFY ALL DIMENSIONS ON SITE. REFER DISCREPANCIES TO THE AUTHOR. THIS DRAWING IS A PRELIMINARY DESIGN AND SHOULD NOT BE RELIED UPON. INDEPENDENT VERIFICATION OF BOUNDARIES SHOULD BE UNDERTAKEN BY CADASTRAL SURVEY.

PROJECT: PROPOSED 2 LOT SUBDIVISION AND TITLE CONSOLIDATION CANAL AND CHARLES STREET LAUNCESTON
FOR: BRILE PTY LTD
DRAWING: SITE PLAN PROPOSED SUBDIVISION AND TITLE CONSOLIDATION
SUBMITTER: R.M.P. **DRAWN:** G.L.M. **CHECKED:** R.M.P.
SCALE: 1:300 **AT A1 SIZE DRAWING SHEET**

PROJECT NO. 14.353 **DRAWING NO. P01** **REV. A**



Mixed Use Development, Charles Street, Launceston

Traffic Engineering Assessment

Job Number: CG107883

Prepared for Five Zero Holdings

20 July 2011



Cardno Victoria Pty Ltd
trading as
Cardno Grogan Richards
ABN 47 106 610 913
150 Oxford Street, Collingwood
Victoria 3066 Australia
Telephone: 03 8415 7777
Facsimile: 03 8415 7788
International: +61 3 8415 7777
cgr@cardno.com.au
www.cardnogroganrichards.com.au

Document Control

Version	Date	Author	Reviewer
F02	20 July 2011	Carlo Morello	CRM Ross Hill

"© Cardno Victoria Pty Ltd All Rights Reserved. Copyright in the whole and every part of this document belongs to Cardno Victoria Pty Ltd and may not be used, sold, transferred, copied or reproduced in whole or in part in any manner or form or in or on any media to any person without the prior written consent of Cardno Victoria Pty Ltd."

Table of Contents

1	Introduction	1
2	Background and Existing Conditions	1
2.1	Location and Land Use	1
2.2	History of the Site	2
2.3	Planning Scheme Zones	2
2.4	Road Network	3
2.4.1	Cimitiere Street	3
2.4.2	Charles Street	5
2.5	Canal Street	6
2.6	Public Transport	6
2.7	Bicycle Network	8
3	Parking and Sustainable Transport Strategy	9
3.1	General	9
3.2	Car Parking Utilisation	9
3.3	Parking Initiatives	9
3.4	Car Parking Vacancies	9
3.5	Policy Changes	9
4	Proposed Development	10
4.1	General	10
4.2	Car Parking	10
4.3	Vehicle and Pedestrian Access	11
4.4	Loading	12
5	Design Considerations	13
5.1	General	13
5.2	Access	13
5.3	Pedestrians	13
5.4	Loading and Garbage Collection	14
5.5	Sight Distance Requirements – Clause 31.8	14
6	Parking Considerations	15
6.1	Loading Requirements – Clause 49	15
6.2	Bicycle Parking Considerations	15
6.3	Car Parking Requirements	16
6.3.1	Launceston Planning Scheme– Clause 48.4	16
6.3.2	Case Study Data	17
6.3.3	Suitability of Proposed Provision	18
7	Traffic Considerations	19
7.1	Traffic Volumes	19
7.2	Existing Site Operation	20
7.3	Previously Approved Development	20
7.4	Traffic Generation	20
7.5	Traffic Distribution	21
7.6	Traffic Volumes	21
7.7	Intersection Analysis	26
7.7.1	General	26
7.7.2	Cimitiere Street/Charles Street	26
7.7.3	Charles Street/Canal Street	28
7.8	Traffic Impact	29
8	Conclusions	29

List of Tables

Table 1:	Proposed Development Floor Areas	10
Table 2:	Sight Distance Requirements – Clause 31.8.....	14
Table 3:	Planning Scheme Loading Requirements – Clause 49.....	15
Table 4:	Planning Scheme Car Parking Requirements – Clause 48.4	16
Table 5:	Peripheral Sales Parking Case Study Data Summary	17
Table 6:	Expected Peak Hour Traffic Volumes.....	21
Table 7:	Rating of Degrees of Saturation	26
Table 8:	SIDRA Intersection Analysis – Cimitiere Street / Charles Street - AM Peak Hour.....	27
Table 9:	SIDRA Intersection Analysis – Cimitiere Street / Charles Street - PM Peak Hour.....	27
Table 10:	SIDRA Intersection Analysis – Future Conditions	28

List of Figures

Figure 1:	Site Location	1
Figure 2:	Schedule 8 - Launceston City Council Planning Scheme Car Parking Cash-In-Lieu Plan 2	2
Figure 3:	Schedule 14 - Launceston City Council Planning Scheme Central Activity District.....	3
Figure 4:	Cimitiere Street, looking south-west beyond the subject site	4
Figure 5:	Cimitiere Street, looking north-east at the intersection with Charles Street.....	4
Figure 6:	Cimitiere Street, looking north at the location it splits from Bathurst Street	4
Figure 7:	Cimitiere Street, looking north-east toward the site and the underpass of Wellington Street	5
Figure 8:	Charles Street, looking south toward the subject site from the north of William Street	5
Figure 9:	Canal Street, looking west along the site's northern boundary	6
Figure 10:	Subject Site Location in Relation to the St John Street Bus Terminus	6
Figure 11:	Bus Routes Serviced by the St John Street Bus Terminus	7
Figure 12:	Proposed Greater Launceston Bicycle Network Map.....	8
Figure 13:	Changes to on-street parking arrangements	10
Figure 14:	Proposed Access Arrangements	11
Figure 15:	Existing Peak Hour Traffic Volumes	19
Figure 16:	Additional Traffic Generated During AM and PM Commuter Peak Hours	23
Figure 17:	Additional Traffic Generated During Weekend Peak Hour.....	24
Figure 18:	Future Expected Traffic Generated During AM and PM Commuter Peak Hours.....	25

Annexes

Annex 1:	Loading Swept Paths
Annex 2:	Concept Access Plan

1 Introduction

Cardno Grogan Richards was retained by Five Zero Holdings to undertake a traffic engineering assessment of the proposed mixed use development on the CH Smith site, Charles Street, Launceston.

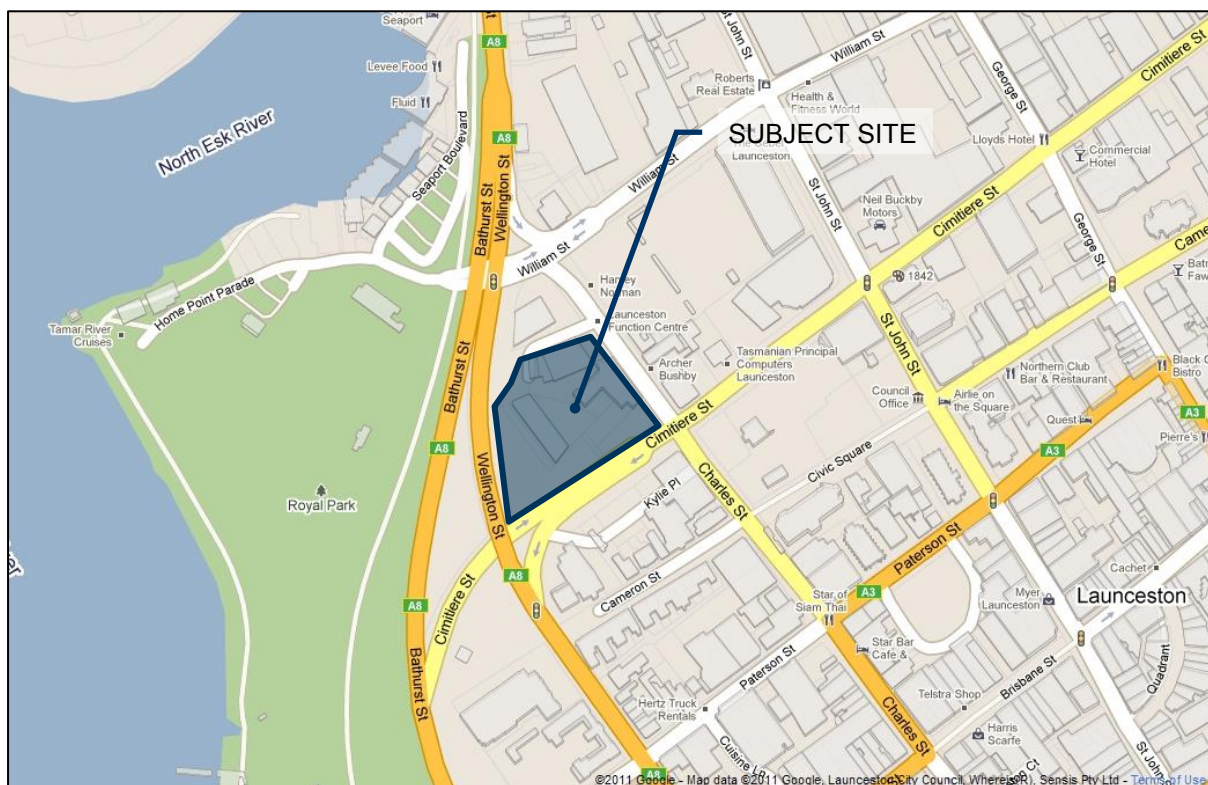
In the course of preparing this assessment, the subject site and its environs have been inspected, plans of the development examined, and all relevant traffic and parking data collected and analysed.

2 Background and Existing Conditions

2.1 Location and Land Use

The subject site is located on the north-west corner of the intersection of Charles Street and Cimitiere Street, Launceston, as shown in Figure 1.

Figure 1: Site Location



The site contains a number of heritage buildings, previously occupied by the Launceston Function Centre, the head office for Tasmania Redline Coaches and C.H Smith Marine. Remaining land is currently utilised as commercial parking areas, with approximately 150 parking spaces provided over two parking areas, including commercial permit parking and customer parking for the CH Smith site.

The site is abutted by Cimitiere Street on the southern boundary, Charles Street along the eastern boundary and Canal Street along the northern boundary. Canal Street provides the primary vehicular access route to the car parking areas on-site, with a crossover to Charles Street also providing access to the site.

Footpaths are provided on Cimitiere Street leading from the commercial car parking area at the south-western corner of the site to the intersection with Charles Street.

2.2 History of the Site

A permit was issued for the site in 2008 which permitted a redevelopment of the site including three levels of car parking, a major gallery, residential apartment building and a five star hotel. The redevelopment included the provision of 149 residential apartments, 518m² of shop floor area, 87 hotel rooms, a function centre/gallery with a floor area of approximately 744m² and 530 on-site car parking spaces, allocated to residents, the hotel and for gallery and public use.

Access to the site was proposed via a new entry from Cimitiere Street and a two-way access at the western end of Canal Street.

2.3 Planning Scheme Zones

The site is located within the area identified by Schedule 8 of the Planning Scheme for the Cash-In-Lieu Car Parking Plan and also within the Central Activities District as specified within Schedule 14 of the Launceston Planning Scheme, as shown in Figure 2 and Figure 3 respectively.

Figure 2: Schedule 8 - Launceston City Council Planning Scheme Car Parking Cash-In-Lieu Plan

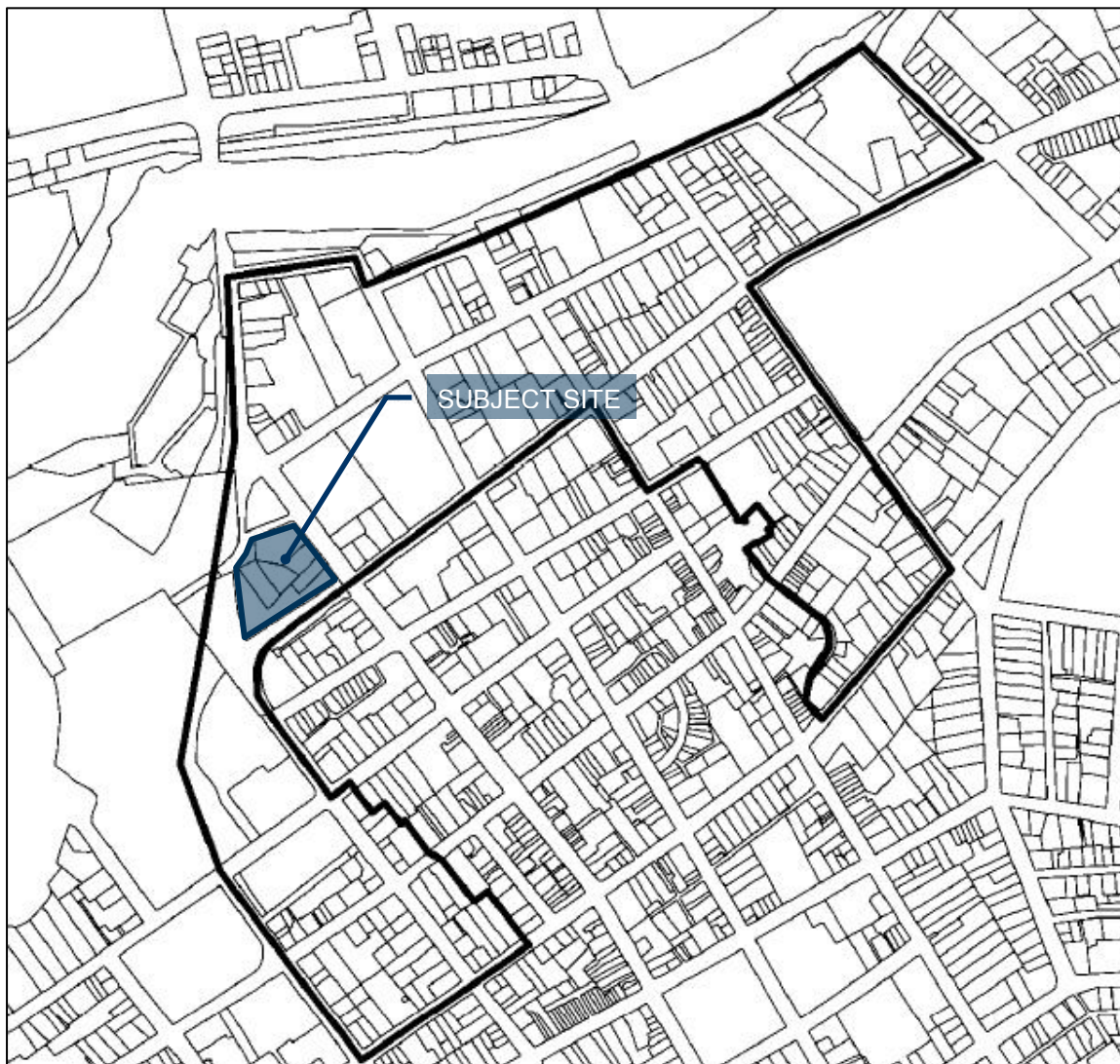
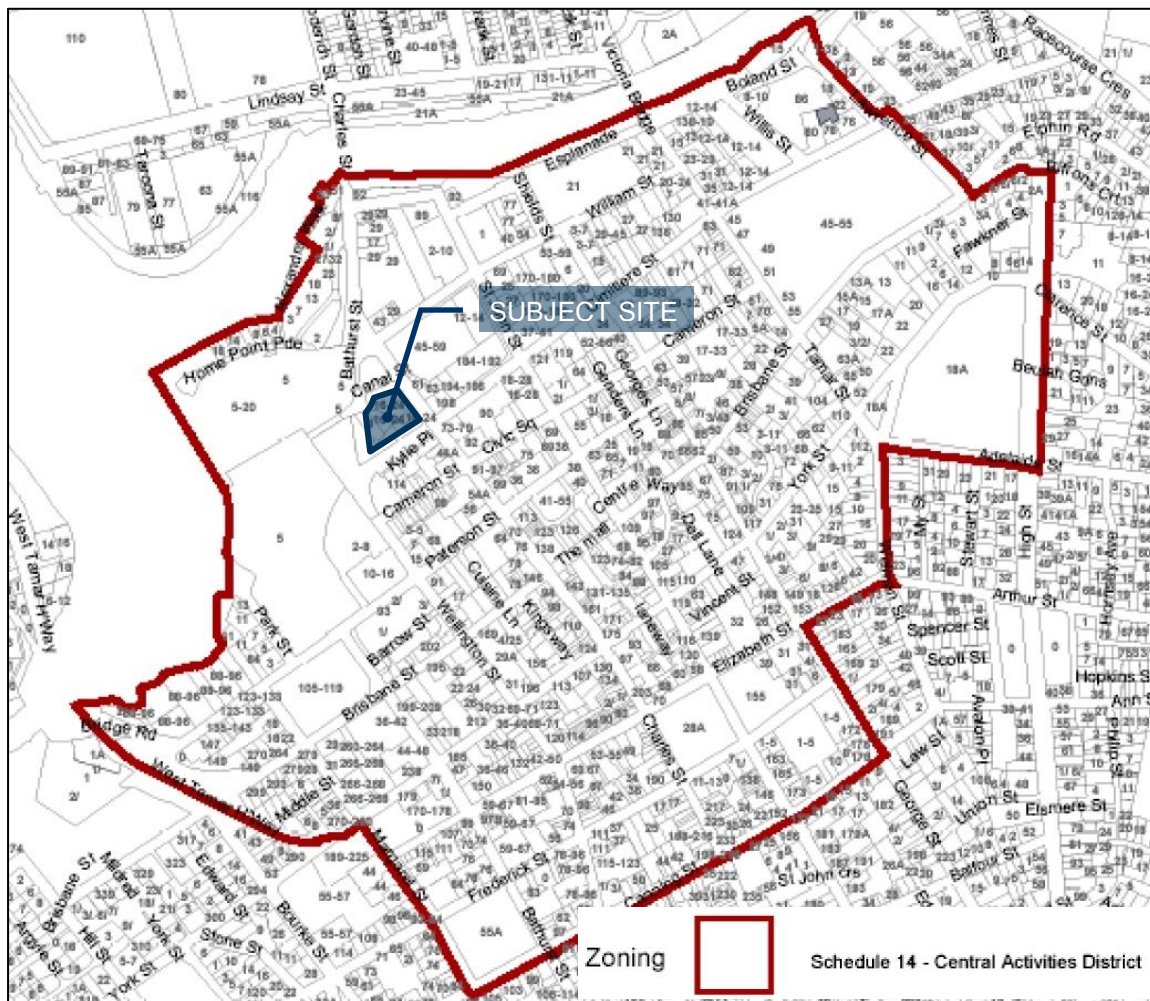


Figure 3: Schedule 14 - Launceston City Council Planning Scheme Central Activity District



2.4 Road Network

2.4.1 Cimitiere Street

Adjacent the site, Cimitiere Street is Class 1 arterial road which becomes a Class 2 Sub Arterial Road approximately mid-block along the site's frontage and operates in a generally south-west to north-east direction. Cimitiere Street operates from Racecourse Crescent in the north-east with a single carriageway and splits at the intersection with Charles Street. The westbound carriageway connects and becomes Wellington Street, travelling southbound, whilst the eastbound carriageway begins as a split from Bathurst Street at the west.

At the frontage of the site, Cimitiere Street provides two lanes in each direction, separated by a central median. At the western corner of the site, the carriageways of Cimitiere Street are grade separated so that the eastbound carriageway can pass beneath Wellington Street as shown in Figure 4.

At the frontage of the subject site, a speed limit of 60kph applies.

Figure 4: Cimitiere Street, looking south-west beyond the subject site



Figure 5: Cimitiere Street, looking north-east at the intersection with Charles Street



Figure 6: Cimitiere Street, looking north at the location it splits from Bathurst Street



Figure 7: Cimitiere Street, looking north-east toward the site and the underpass of Wellington Street



2.4.2 Charles Street

Charles Street, at the frontage of the site, is a local road which operates in a generally north-west to south-east direction from William Street in the north-west to Howick Street in the south-east, where it continues as Charles Street South. At the frontage of the site, Charles Street operates with a single lane and parallel kerbside parking in each direction.

At the intersection with William Street, traffic islands restrict movements to left-in and left-out of Charles Street only, as shown in Figure 8. The intersection with Cimitiere Street is fully directional, with a short kerbside lane in the southbound direction providing two lanes on this approach.

Figure 8: Charles Street, looking south toward the subject site from the north of William Street



2.5 Canal Street

Canal Street operates from Charles Street, along the northern border of the site and provides access to the car parking at the rear of the site. Canal Street accommodates two-way traffic, with kerbside parking permitted on the southern side of the road. Access is also provided to the AAMI site on the northern side of Canal Street, as shown in Figure 9 below.

Figure 9: Canal Street, looking west along the site's northern boundary



2.6 Public Transport

Public transport in the vicinity of the site is limited to bus services, with the St John Street Bus Terminus located approximately 500 metres to the south-west of the site between Paterson Street and York Street.

The location of the St John Street bus terminus in relation to the subject site and a list of the bus services available from the St John Street Bus Station are shown in Figure 11 and Figure 10.

Figure 10: Subject Site Location in Relation to the St John Street Bus Terminus

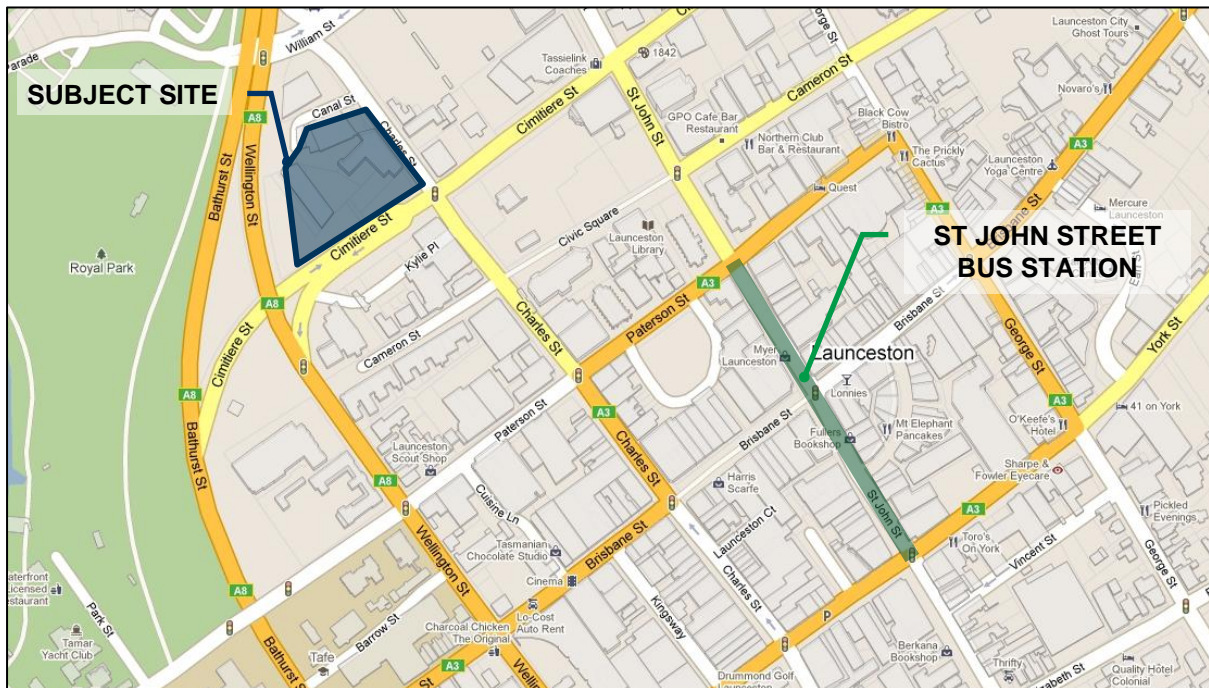
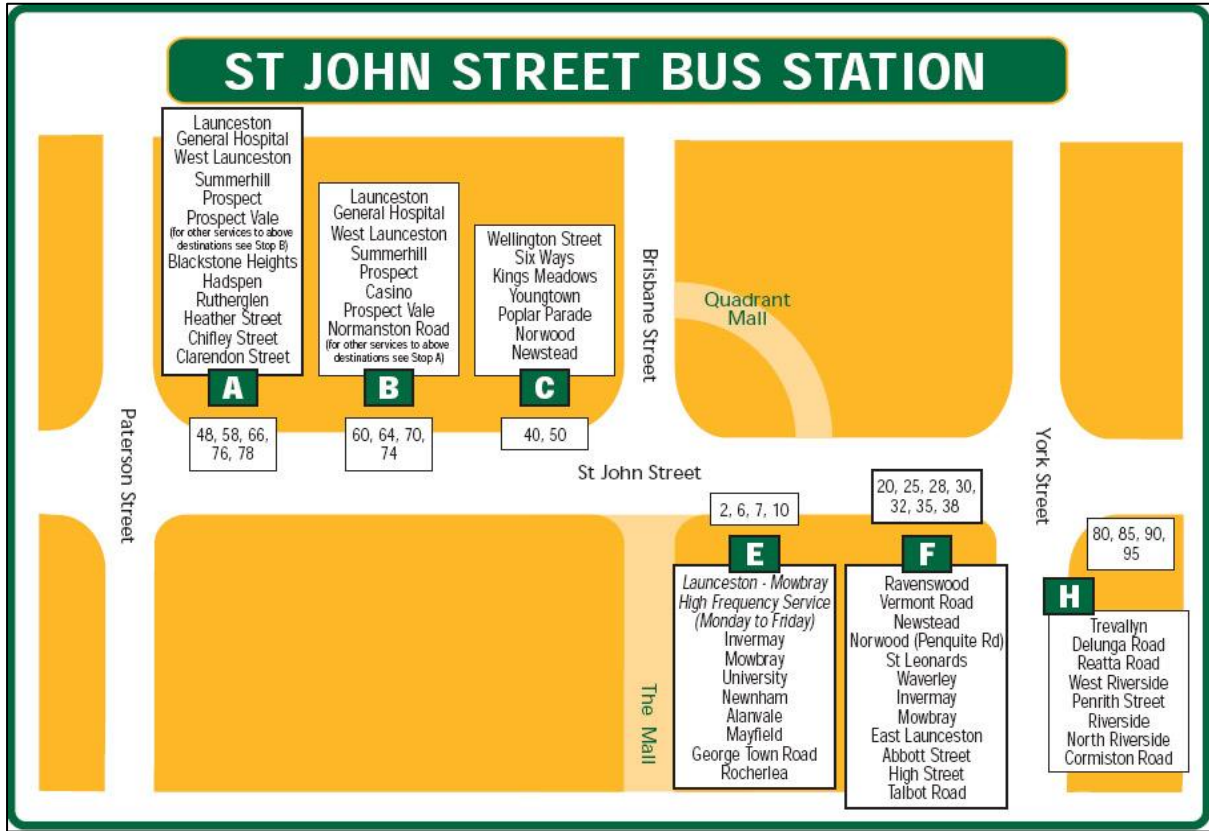


Figure 11: Bus Routes Serviced by the St John Street Bus Terminus

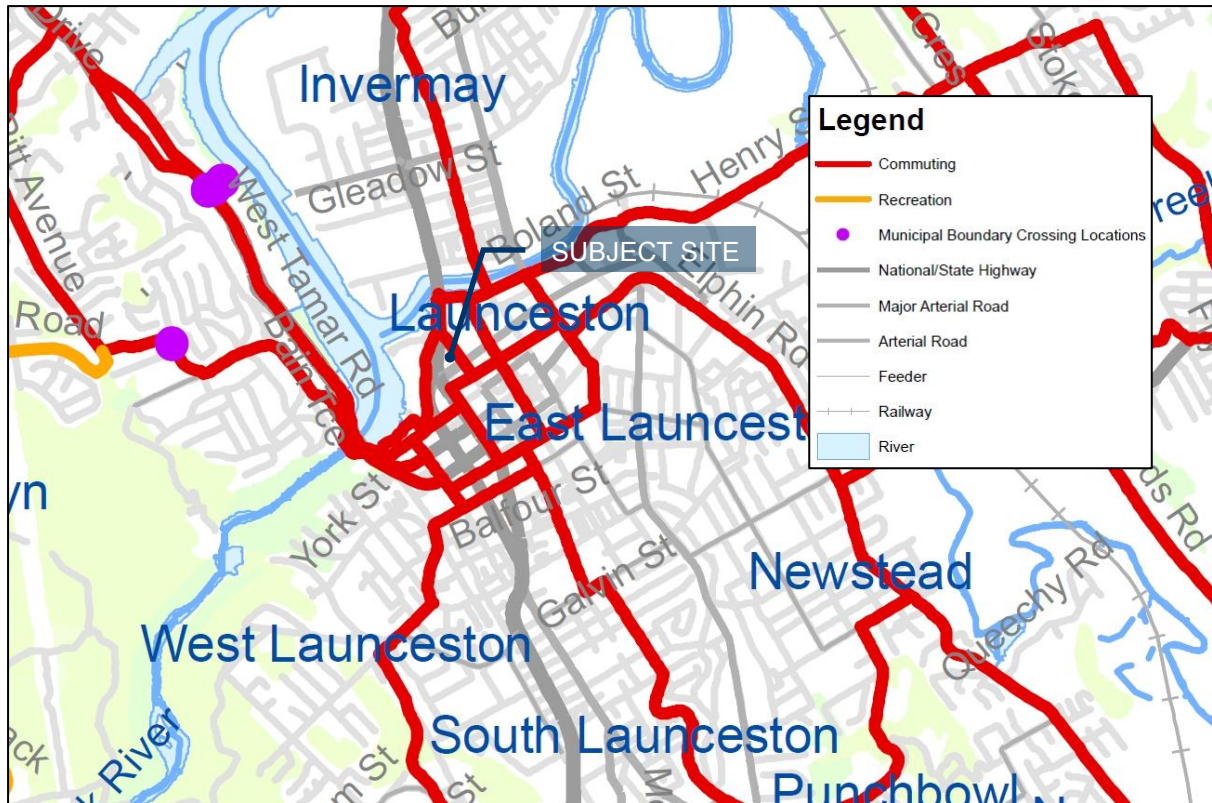


2.7 Bicycle Network

Reference to the Greater Launceston Bicycle Commuting Network Map indicates that a number of roads in the vicinity of the site have been identified as future commuter routes including Charles Street, Paterson Street, St John Street and Brisbane Street.

Figure 12 shows an excerpt of the Greater Launceston Bicycle Network Map.

Figure 12: Proposed Greater Launceston Bicycle Network Map



3 Parking and Sustainable Transport Strategy

3.1 General

In August 2009, Luxmoore Parking Consulting prepared the Parking and Sustainable Transport Strategy for the City of Launceston to review the current objectives and planning regulations regarding parking and sustainable transport modes.

3.2 Car Parking Utilisation

The study included a revision of the existing supply and demand of car parking in the Launceston CAD. Namely, the study identified the following:

- There are approximately 4,391 car parking spaces in Launceston CAD, including 2,773 spaces off-street and 3,787 paid-parking spaces.
- Average utilisation of parking in the CBD was determined by Council's parking department and was based on yearly revenue, with an average utilisation of between 61.3% and 66.4% between 1997-2004.

3.3 Parking Initiatives

The study identified a number of initiatives for the City of Launceston to achieve its sustainability goals, which included regulating car parking restrictions and provisions, encouraging remote commuter parking, a reduction in the minimum parking requirements for developers and introducing a cap on the maximum number of spaces that may be provided in a specific area.

In particular, the following initiatives were identified within the study

- Regulate parking through time restrictions and user restrictions;
- Regulate parking on traffic routes with potential clearway routes;
- Share parking dependent on uses and temporal demands;
- Implementation of a Parking Brokerage Service for businesses to share, lease or sell excess parking;
- Provide improved user information on the Launceston Council website regarding parking restrictions, locations and costs;
- Encourage remote parking;
- Improve pedestrian and cyclist facilities;
- Provide a cash-in-lieu parking scheme;
- **Reducing the parking supply by reducing minimum parking requirements and/or establishing a cap on the total parking for an area or development.**

3.4 Car Parking Vacancies

Surveys undertaken on Monday, Wednesday and Fridays in August and September 2008 were provided within the study and identified the peak parking utilisation on Friday 5th September 2008, when approximately 87% of parking was utilised within the Council CBD Car parks.

3.5 Policy Changes

As a result, the study recommended a number of policy changes were recommended, including:

- For developments providing over 40 spaces, they must justify the provision and provide a Travel Plan for the development; and
- Recommendation that the car parking exemption zone be extended to include the whole of the Central Activities District.

4 Proposed Development

4.1 General

It is proposed to redevelop the site for the purposes of a mixed use development, containing peripheral sales, business, restaurant and other land uses as described within Table 1.

Table 1: Proposed Development Floor Areas

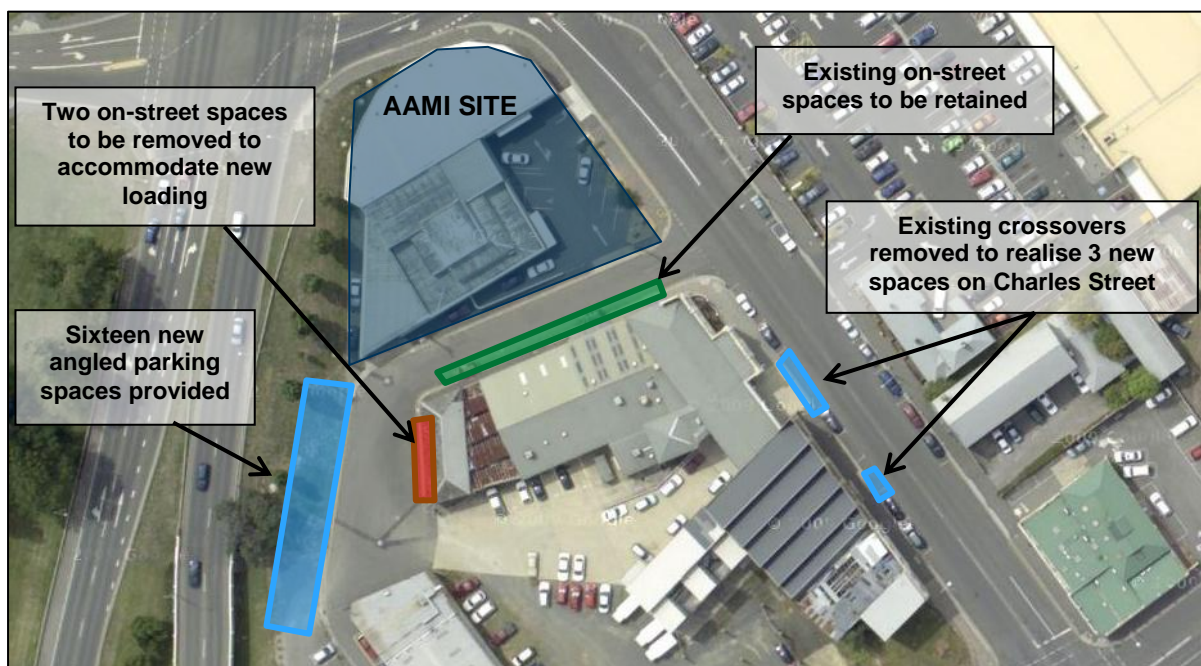
Use	Floor Area
Peripheral Sales	9,391 m ²
Restaurant	1,347m ²
Business Premises	274 m ²
Place of Assembly	254 m ²
Vehicle Parts and Sales	369 m ²
Ancillary Uses	528 m ²
Total	12,163 m²

4.2 Car Parking

A total of 254 car spaces are proposed in two car parking areas - one smaller area, providing 35 spaces and accessed from Cimitiere Street, approximately mid-block along the sites abuttal, and a second larger car park, providing 219 spaces, accessed from Cimitiere Street (entry only) and Canal Street (entry and exit).

A further 16 on-street parking spaces are proposed on the north-west side of Canal Street, adjacent to Wellington Street, with two existing on-street parking spaces removed on the south-east side of Canal Street to accommodate the new loading arrangements. Remaining parking along the south side of Canal Street is to be retained as existing, as detailed in Figure 13.

Figure 13: Changes to on-street parking arrangements



It is further noted that site has two access points to Charles Street, a single crossover to the southern end of the site, and a double crossover to the northern end of the site. It is proposed to remove these existing vehicle crossovers to Charles Street and reinstate as kerb and channel. As a result, it is anticipated that up to an additional three spaces could be realised on-street on Charles Street.

In total, 271 parking spaces are proposed to be associated with the development, inclusive of new and removed on-street parking spaces.

4.3 Vehicle and Pedestrian Access

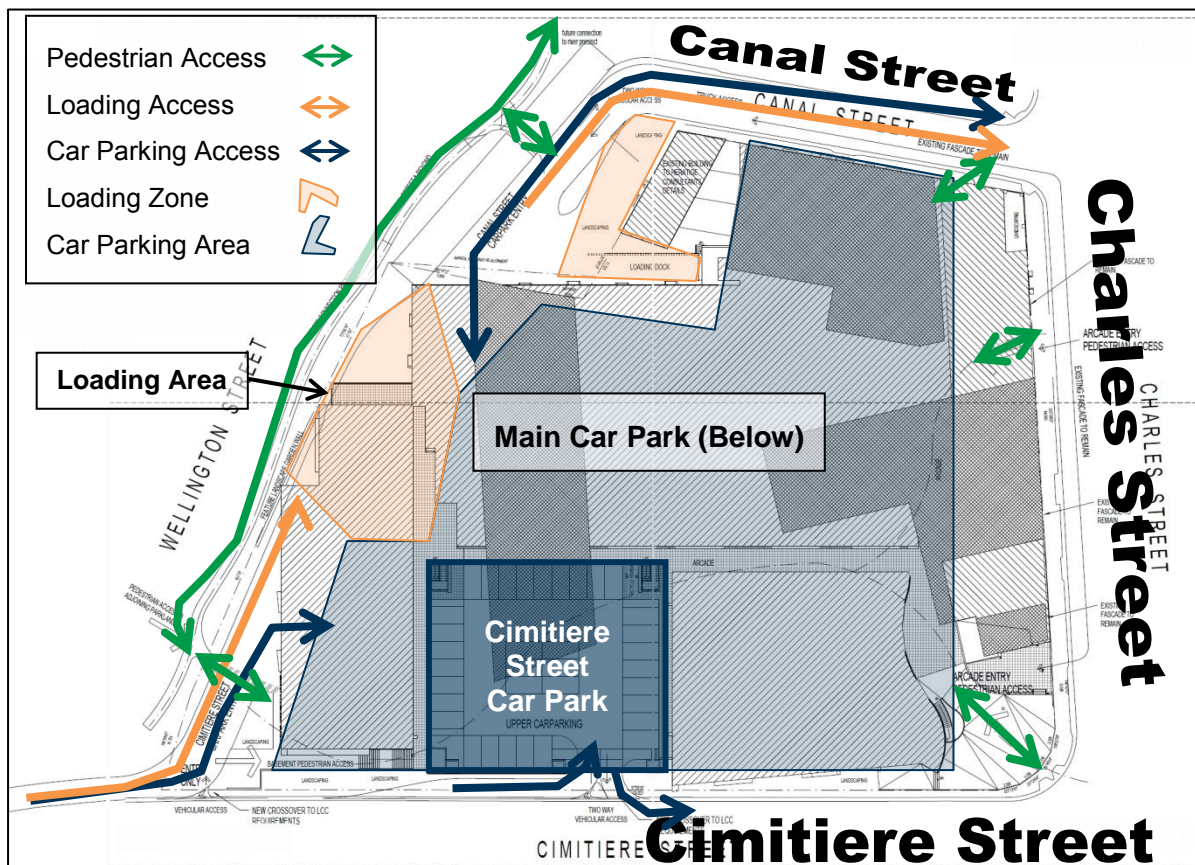
Primary passenger vehicle access for the main parking area is to be from Canal Street, which is to operate two-way. A new left-in only access to Cimitiere Street will provide entry only into the main car park with access from Cimitiere Street to be facilitated by the provision of a new left-turn deceleration lane entering the site.

A new shared pedestrian and cycle path is proposed along the western boundary of the site to link with properties at the north of the site. A pedestrian link is proposed from this new shared path to the basement and ground access points.

A new access to Cimitiere Street is proposed approximately midblock along the site boundary.

Figure 14 shows the proposed access locations for the development.

Figure 14: Proposed Access Arrangements



4.4 Loading

Two loading zones are proposed on the site, as shown in Figure 14, which can accommodate 19.0m semi trailer vehicles. Primary ingress for loading vehicles is proposed from the new Cimitiere Street access and the proposed left turn deceleration lane has been designed to accommodate the swept paths for a 19.0m semi. An access driveway is to operate from the Cimitiere Street access to Canal Street, with a section providing loading vehicle access only to provide separation between commercial and .

The northern loading area is to allow semis to drive forwards into the loading area, prop whilst loading via a raised loading dock at the rear, and then exit in a forwards direction to Canal Street. A raised island is provided between the loading area and the main car parking access from Canal Street to provide separation between loading and access.

5 Design Considerations

5.1 General

The Launceston Planning Scheme specifies that car parking is to be designed in accordance with the dimensions specified within the Australian Standard for Off-Street Car Parking. AS/NZS2890.1:2004 specifies different parking dimensions and modules dependent on user types and classes. The Launceston Planning Scheme specifies that for peripheral sales developments, the provided parking should be designed to accommodate medium-term parking. For the proposed development and use, it is considered appropriate to provide parking in accordance with the specifications for User Class 3 as specified within the Australian Standard.

Spaces are generally 2.6m wide, 5.4m long and have a minimum aisle width of 6.2m in accordance with the specifications for User Class 3. Spaces next to walls have been widened to 2.8m wide in accordance with the Standard.

Disabled bays have been designed generally in accordance with AS/NZS2890.6:2009, with a dedicated shared space on one side to facilitate access. In accordance with Clause 48.8 of the Launceston Planning Scheme, a footpath of minimum width 1.5m is provided between the disabled bays and the access to the principal building, and in the case of the undercroft parking, the main access being Lift 1.

Head clearance within the undercroft car park has been provided in accordance with AS/NZS2890.1:2004, being a minimum 2.2m, and also in accordance with AS/NZS2890.6:2009, being a minimum 2.5m above disabled bays.

The envelope for car parking spaces provided within AS/NZS2890.1:2004 specifies that columns at the open end of spaces (entry end) should be located within 750mm-1750mm from the open end. Columns are generally 1.2m long, and therefore encroach slightly on the envelope specified within the Standard, however it is not expected that access or door opening will be significantly impacted on.

5.2 Access

Access driveways have been designed with a minimum width of 3.6m between obstructions for one-way access and 6.1m between obstructions for two-way accesses, in accordance with AS/NZS2890.1:2004 and in excess of Clause 48.7 of the Launceston Planning Scheme.

Left turn deceleration lanes are to be provided at the entries to the site from Cimitiere Street as per the Concept Layout Plans at Annex 2, and these have been designed generally in accordance with AustRoads Guidelines.

5.3 Pedestrians

Pedestrian accesses have been provided to the site in accordance with Clause 48.8 of the Launceston Planning Scheme, being a minimum 1.0m wide. Internally pedestrian access is facilitated by a number of walkways to/from car parking which have been provided with a minimum width of 1.2m wide.

5.4 Loading and Garbage Collection

The swept path diagrams attached at Annex 1 demonstrate access to the site and each of the loading areas, indicating that the western loading area can accommodate one 19.0m semi trailer and a 12.5m truck simultaneously.

It is recommended that car parking spaces located adjacent the access and loading areas are allocated to staff only so that the turnover of these spaces is reduced and there is reduced interaction with loading vehicles.

5.5 Sight Distance Requirements – Clause 31.8

Clause 31.8 of the Launceston Planning Scheme specifies sight distance requirements for new vehicular access points, with the following minimum and preferred distances required in an urban environment.

Table 2: Sight Distance Requirements – Clause 31.8

Design Speed (km/h)	ESD (m) – preferred	SISD (m) – minimum
40	100	60
50	125	80
60	160	105
70	220	130
80	305	165

ESD = Entering Sight Distance

SISD = Safe Intersection Sight Distance

Sight time recordings undertaken on-site at the proposed western ingress point from Cimitiere Street suggest that the access point is visible 6.9 seconds (on average) before a vehicle reaches the access point. Assuming that vehicles are travelling at the posted speed limit on Bathurst Street, this equates to a sight distance of approximately 115 metres.

Furthermore, a review of the approximate location of the proposed upper car park access to Cimitiere Street indicates that sight lines from the proposed access point are expected to extend for in the order of 170m-180m.

The above distances exceed the minimum sight distance requirements shown in Table 2, and are considered to be appropriate. Additionally, whilst Bathurst Street is in a 60km/h zone, Cimitiere Street is unsigned, and therefore adopts the General Urban Speed Limit of 50km/h, and with an uphill approach to a signalised intersection, traffic will generally be slowing on approach to the site.

The locations of the proposed western ingress to Cimitiere Street and the left-in/left-out access to the upper car park from Cimitiere Street are therefore considered to satisfy the sight distance requirements of the Launceston Planning Scheme.

6 Parking Considerations

6.1 Loading Requirements – Clause 49

Clause 49 of Launceston Planning Scheme outlines the requirements for the loading and unloading of vehicles. It specifies that:

- The driveway to the loading bay is at least 3.6 metres wide. If a driveway changes direction or intersects another driveway, the internal radius at the change of direction or intersection must be at least 6 metres.
- A loading bay is provided on the land for loading and unloading vehicles as specified in Table 3.

Table 3: Planning Scheme Loading Requirements – Clause 49

Floor Area of Building	Minimum Loading Bay Dimensions	
2,600 m ² or less in single operation	Area	27.4 m ²
	Length	7.6 m
	Width	3.6 m
	Height clearance	4.0 m
For every additional 1,800 m ² or part	Additional 18 m ²	

Considering the above, the proposed development requires a loading area of approximately 112m². The proposed loading areas exceed all minimum dimension requirements and are considered to satisfy the specifications of the Launceston Planning Scheme.

6.2 Bicycle Parking Considerations

The Launceston Planning Scheme does not specify parking for bicycle parking and trip end facilities, however in an effort to reduce the reliability on passenger vehicles and therefore the car parking and traffic impact of the proposal, it is recommended that bicycle parking and facilities be provided for the proposal.

Demand for parking is generally a function of supply, and therefore the provision of secure bicycle parking and trip end facilities, such as staff changerooms and showers, will encourage staff to utilise bikes to commute to work, and therefore reduce the need for long term car parking for staff. It is therefore recommended that secure bicycle parking be provided for staff, complemented by staff changerooms, lockers and showers, and parking for visitors be provided at the periphery of the site, in areas which can easily be accessed from the street frontages.

It is recommended that bicycle parking be designed generally in accordance with the Australian Standard AS/NZS2890.3:2001.

Reference to Section 2.7 indicates that Charles Street is identified as a future commuter route in the Launceston Bicycle Network, and therefore future facilities on Charles Street will also encourage staff and visitors to travel to the site on a bike, rather than make the trip in a car.

This initiative is also in-line with the Parking and Sustainable Transport Strategy for the City of Launceston.

6.3 Car Parking Requirements

6.3.1 Launceston Planning Scheme– Clause 48.4

Clause 48.4 of the Launceston Planning Scheme specifies the following parking provision requirements with regard to the different components of the proposed development.

Table 4: Planning Scheme Car Parking Requirements – Clause 48.4

Component	Area/No	Requirement	
		Rate	Total
Peripheral Sales	9,391 m ²	4 spaces per 100m ² (medium term)	375.6
Restaurant	1,347m ²	1 space per 6m ² of floor area (medium term*)	224.5
Business Premises	274 m ²	1 space per 40m ² of floor area (medium term*)	6.9
Place of Assembly	254 m ²	1 space per 10m ² of floor area (medium term [^])	25.4
Vehicle Parts and Sales	369 m ²	1 space per 25m ² of retail floor area (short term)	14.8
Total			648

* Where 5 or more spaces are required for these uses, one space shall be for persons with disability, and one additional space per 100 spaces thereafter.

[^] For Place of Assembly uses, a single disabled bay must be provided, plus an additional space per 100 spaces

Based on the disabled parking requirements for a portion of the uses, at a rate of 1 space per 100 spaces, the proposed development is therefore considered to require the provision of 3 disabled bays.

The proposed provision of 271 spaces associated with the proposal therefore represents a shortfall of 377 spaces when considering the Planning Scheme requirements.

The proposed provision of 3 disabled bays is in accordance with the Planning Scheme requirements.

Furthermore, the responsible authority must consider a number of additional factors in assessing the parking provision of a development, including:

- the character of the area and of the development, including traditional parking patterns;
- the nature and size of the development;
- the availability and capacity of on-street and other off-street parking facilities;
- the role of the access street in the road hierarchy;
- any local traffic or parking management plans;
- the safety of traffic, pedestrians and users of the subject land;
- the availability of public transport; and
- the surrounding land use and zoning.

6.3.2 Case Study Data

Case study data held by Cardno and other consultancies for similar developments in various locations, including Victoria, Sydney and Queensland indicates peak parking rates ranging from 0.88 to 3.5, with an average peak demand of 1.64 spaces per 100m² floor area as outlined in Table 5.

Table 5: Peripheral Sales Parking Case Study Data Summary

Location	Peak Parking/100m ²
Big W, Wonthaggi	2.73
Officeworks, Frankston	1.90
Ashmore Showplace and Spotlight, Ashmore, QLD	1.31
Epping 2 Homemaker Centre	0.92
The Warehouse, Coolaroo	1.47
Guests Furniture, Dandenong	0.52
Anderson's Furniture Store, Dandenong	0.38
Homemaker Centre, Ringwood	1.21
Bunnings Warehouse, Epping	1.23
Guests Furniture Store, Nunawading	1.07
Kmart, Campbellfield	3.5
Fantastic Furniture and Plush Leather, Dandenong	0.88
Big W, Wonthaggi	3.5
Harvey Norman Hardware House, Nunawading	1.35
Harvey Norman, Balgowlah, Sydney	1.91
Harvey Norman Furniture, Auburn, Sydney	1.4
Harvey Norman Electrical, Auburn, Sydney	2.85
Harvey Norman, Wiley Park, Sydney	2.0
Harvey Norman, Miranda, Sydney	1.3
Harvey Norman, Moorabbin	1.38
Forty Winks, Cheltenham	1.44
Harvey Norman, Moorabbin	1.70
Clive Peeters Electrical, Dandenong	3.08
Northland Homemaker Centre, Murray Road, Preston	1.7
Harvey Norman/ Office Works, Bell Street, Preston	1.35
Drummond Golf/Ray Tent City, Bell Street, Preston	1.26
Myer Megamart, Settlement Road, Thomastown	0.88
Epping Homemaker Centre, High Street, Epping	0.92
Average	1.61

6.3.3 Suitability of Proposed Provision

An outcome of the Launceston Parking and Sustainable Transport Strategy was that policy regarding car parking in the Launceston CAD be amended so that the car parking exemption zone be extended to the whole of the CAD and that maximum restrictions be placed on developments within central areas.

The proposed development falls within the Central Activities District and is currently located immediately adjacent the boundary of the car parking exemption zone.

Given the site's location, the proposed provision of a reduced car parking supply is therefore considered to be in accordance with the recommendations of the Parking and Sustainable Transport Strategy.

Furthermore, a review of case study data for similar developments indicates that the proposed parking provision is likely to accommodate the peak parking demands generated by the site.

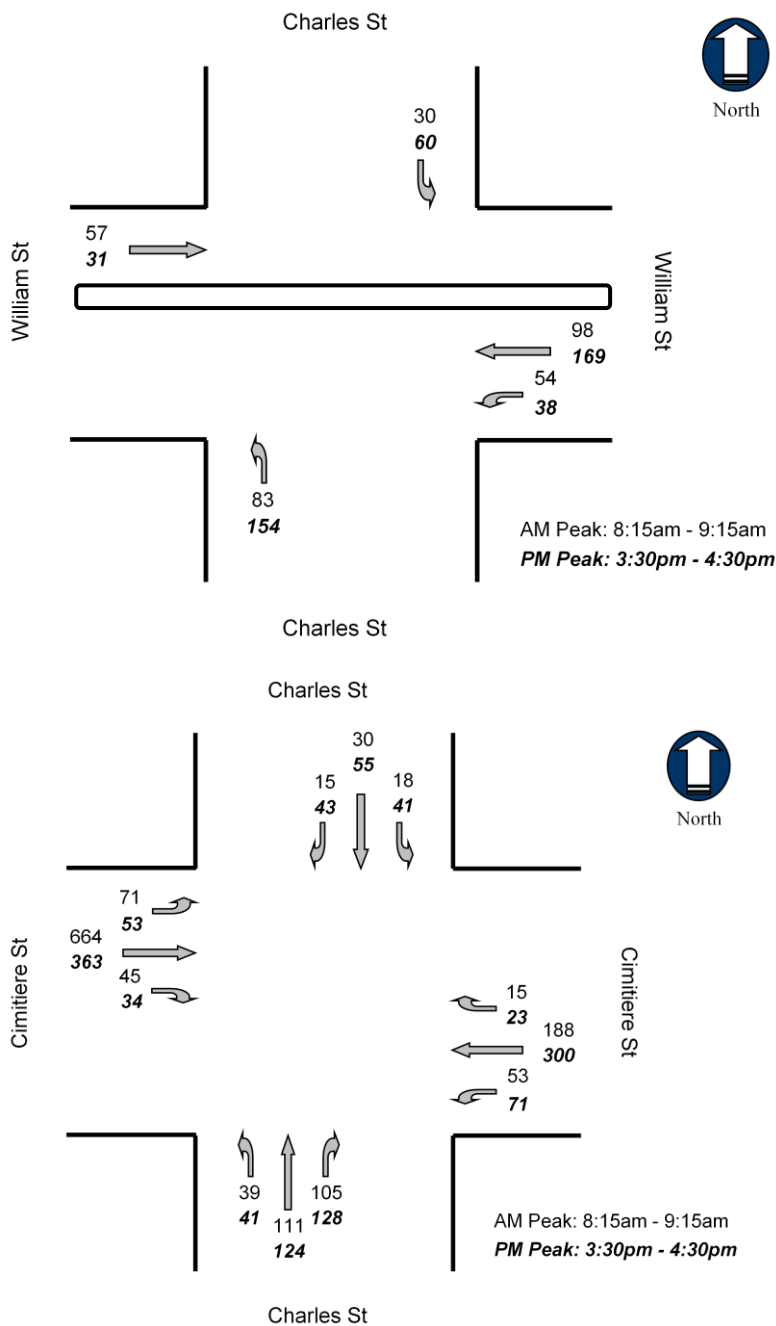
7 Traffic Considerations

7.1 Traffic Volumes

Based on discussions with Council Officers, who indicated that the most important period for the surrounding road network was during the weekday AM and PM peak periods, turning movement counts were undertaken by Trans Traffic Surveys on behalf of Cardno at the intersections of Cimitiere Street/Charles Street and Williams Street/Charles Street, on Thursday 23rd June 2011 between 7:00am and 10:00am, and between 3:30pm and 6:30pm.

The AM peak hour was found to occur between 8:15am-9:15am with the PM peak hour occurring between 3:30pm-4:30pm. The peak hour results of the surveys are shown in Figure 15.

Figure 15: Existing Peak Hour Traffic Volumes



7.2 Existing Site Operation

The site currently provides approximately 150 spaces within the commercial and other car parking areas. The existing parking on-site is generally long term, with the majority of spaces expected to fill during the morning peak hour, and vacate during the afternoon peak hour, potentially generating a vehicle movement in the respective directions during these peaks.

7.3 Previously Approved Development

It is noted that the previously approved proposal for the site involved the development of 149 residential apartments, 518m² of shop floor area, 87 hotel rooms, 744m² of function/gallery floor area and 530 on-site car parking spaces. Parking on-site was to be allocated to residents, the hotel and for gallery and public use.

With a parking provision of approximately twice that is now proposed, it is expected that the previously approved development on the subject site would generate significantly greater traffic than the current proposal.

7.4 Traffic Generation

Peak traffic generation for peripheral sales developments similar to the proposal typically occurs on Saturdays around the lunch time period. However, the most critical time is often on a Friday afternoon when development generated traffic coincides with the commuter peak period.

Case study data in relation to traffic generation for similar premises has been sourced for both Saturdays and Friday afternoons which indicates traffic generation at an overall rate of 4 movements and 2 movements per 100 square metres during the Saturday and Friday peaks respectively.

The proposed 11,635 square metres of floor area (not including the ancillary and atrium areas) is therefore expected to generate 465 movements and 233 movements in the Saturday and Friday PM peak hours respectively. These traffic movements are generally split evenly between inbound and outbound movements during the peaks.

It is noted that the case study data adopted above was recorded at developments which generally provided parking at a rate in excess of that provided by the proposed development. The proposal could therefore be expected to generate fewer vehicle movements during the peak hours due to the reduced on-site parking provision.

Based on discussions with Council Officers, it is understood that the most important periods for traffic generation of the site are considered to be on a weekday morning and afternoon. With regard to the expected traffic generation of the proposal during the weekday AM peak hour, it is anticipated that this will be generally associated with staff arriving at the site. For the purposes of analysis, it has been assumed that during the weekday AM peak hour, the site will generate approximately 10% of the PM peak hour traffic, with the assumption that 90% of traffic movements are expected to be inbound.

7.5 Traffic Distribution

The site is located at the northern end of the Launceston City Centre, and based on the distribution of the residential, industrial and commercial sectors surrounding the site, it is expected that the primary catchment area for the proposal will be from the south.

With consideration of the site location, existing traffic conditions, the proposed access arrangements and existing traffic distributions, it is expected that during peak periods the traffic generated at the site would be split as follows:

- Approximately 60% of traffic will access the site from the south via Cimitiere Street or Charles Street;
- Approximately 20% of traffic will access the site from the east via Cimitiere Street;
- The remaining 20% of traffic will be generated from the north, accessing the site via Charles Street;
- Approximately 90% of vehicles accessing the site will be utilising the larger car park accessed from either Cimitiere Street or Canal Street, with the remaining 10% of vehicles utilising the smaller car park accessed from Cimitiere Street; and
- All access from Cimitiere Street from the east, and from Charles Street, will be via Canal Street.

7.6 Traffic Volumes

Based on the preceding, the expected traffic volumes generated by the proposal during the peak hours is provided within Table 6, with the expected traffic volumes provided within Figure 16.

These expected traffic volumes have been superimposed onto the existing traffic volumes and the future road network volumes are shown in Figure 18.

For the purposes of information, the expected traffic generation and additional traffic movements generated by the proposal are provided within Figure 17.

Table 6: Expected Peak Hour Traffic Volumes

Component	Inbound	Outbound	Total
AM Peak	105	12	116
PM Peak	116	116	233
Weekend Peak	232	233	465

With reference to Figure 16, Figure 18 and Figure 17, it is expected that the proposal will generate an additional one vehicle movement to the intersection of Wellington Street/William Street/Bathurst Street during the AM peak hour, and up to 25 vehicle movements during the PM peak hour. This equates to an average of less than one additional vehicle movement every two minutes during the PM peak hour, or approximately one vehicle movement every signal cycle, which is considered to be low in engineering terms and will have a negligible impact on the operation of this intersection.

During both weekday peak periods, the proposal is expected to generate no more than an additional 19 movements to the right turn into Charles Street at Cimitiere Street east and to the right turn from Charles Street into Cimitiere Street west, equating to less than one additional vehicle movement every cycle to each of these movements.

The majority of traffic exiting the site during the peak periods will be via the right turn into Charles Street from Canal Street, with approximately 82 vehicle movements expected during the PM peak hour. Given the relatively low volumes on Charles Street during the peaks, this level of traffic is expected to be accommodated by the current sign controlled intersection.

During the weekend period, the proposal is expected to generate an additional 51 vehicle movements to the intersection of Wellington Street/William Street/Bathurst Street. An additional 38 vehicle movements are expected to each of the right turns from Cimitiere Street east and Charles Street north, equating to an average of approximately an additional three vehicle movements every five minutes to these movements. Based on discussions with Council Officers is expected to be accommodated at this intersection with no significant impacts to its operation.

The location of the signals immediately west of Charles Street on William Street and the existing flows on William Street westbound during the peak periods are expected to provide sufficient gaps and capacity for the additional vehicles generated to the left turn from Charles Street into William Street during all peak periods.

It is noted that the adopted approach is considered to be conservative as it assumes that all traffic generated by the proposal are new trips to the road network, when in reality as the site currently operates as a commercial car park, some of the traffic generated by the proposal will likely result in the redistribution of traffic rather than the generation of new trips to the surrounding road network.

Figure 16: Additional Traffic Generated During AM and PM Commuter Peak Hours

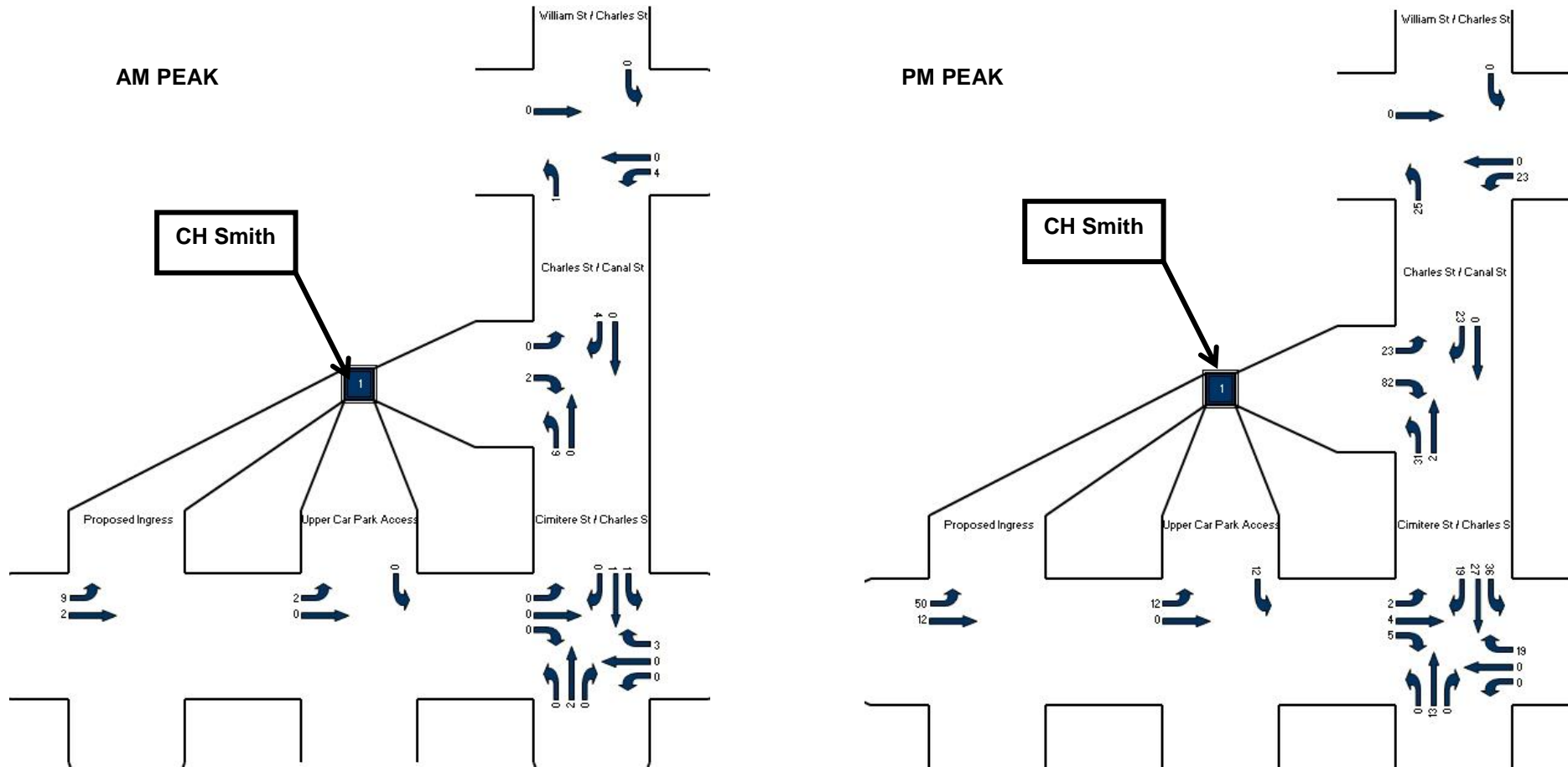


Figure 17: Additional Traffic Generated During Weekend Peak Hour

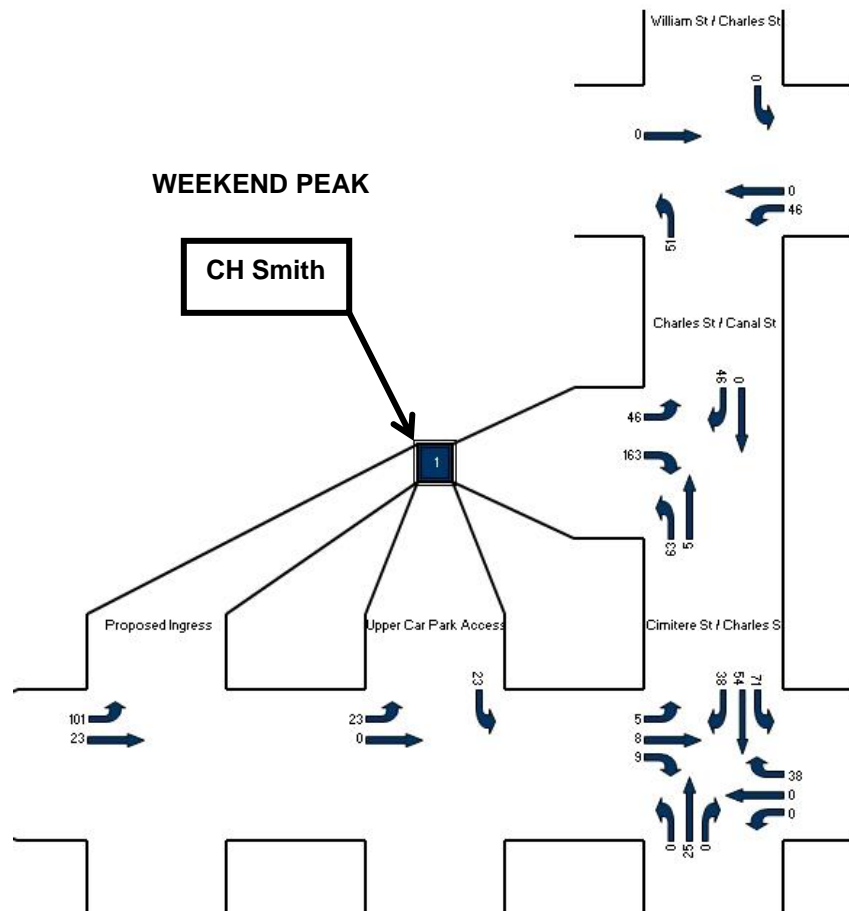
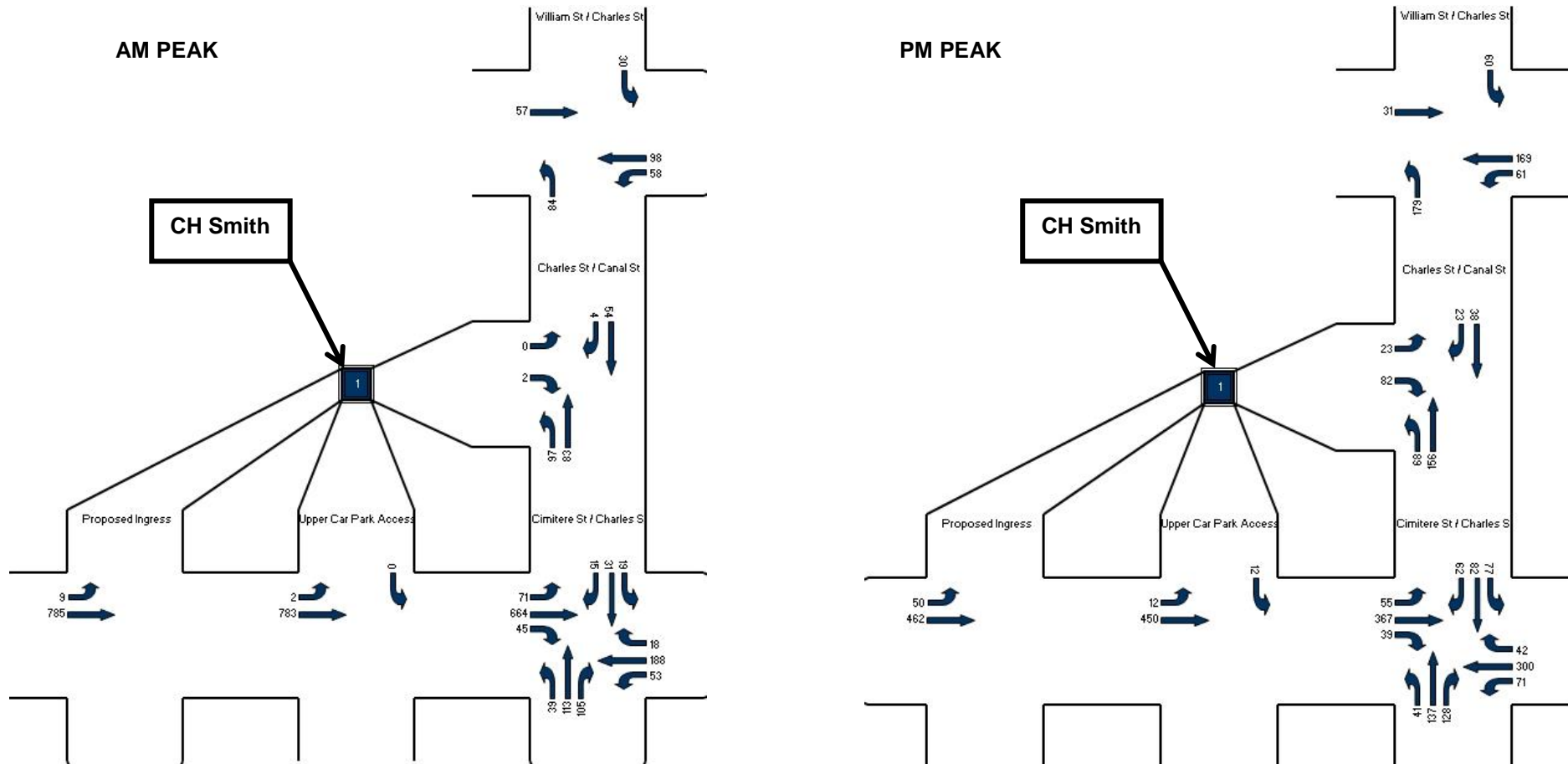


Figure 18: Future Expected Traffic Generated During AM and PM Commuter Peak Hours



7.7 Intersection Analysis

7.7.1 General

Notwithstanding the preceding, for the purposes of analysis and to determine the impact of the proposal on the operation of the intersections of Cimitiere Street/Charles Street and Charles Street/Canal Street, these intersections have been analysed using SIDRA Intersection, adopting the AM and PM peak hour volumes provided within the previous sections. These periods have been identified by Council as the most important periods.

This computer package, originally developed by the Australian Road Research Board, provides information about the capacity of an intersection in terms of a range of parameters, as described below:

Degree of Saturation (D.O.S.) is the ratio of the volume of traffic observed making a particular movement compared to the maximum capacity for that movement. Various values of degree of saturation and their rating are shown in Table 7:

Table 7: Rating of Degrees of Saturation

D.O.S.	Rating
Up to 0.6	Excellent
0.6 to 0.7	Very Good
0.7 to 0.8	Good
0.8 to 0.9	Fair
0.9 to 1.0	Poor
Above 1.0	Very Poor

The **95th Percentile (95thile) Queue** represents the maximum queue length, in metres, that can be expected in 95% of observed queue lengths in the peak hour; and

Average Delay is the delay time, in seconds, which can be expected over all vehicles making a particular movement in the peak hour.

7.7.2 Cimitiere Street/Charles Street

The operation of the intersection of Cimitiere Street/Charles Street has been modelled using SIDRA Intersection for both the existing and future conditions for both the AM and PM weekday peak periods, with the results provided within Table 8 and Table 9.

Reference to the results of the SIDRA analysis indicates that the intersection currently operates under 'excellent' operating conditions during both the AM and PM peak periods, and will continue to do so with the additional traffic volumes generated by the proposal. In fact, the analysis indicates that during both peak periods, the intersection Degree of Saturation will increase by less than 0.05 and, with the exception of the Charles Street north approach during the PM peak, each movement DoS will increase by no more than 0.1, which is considered to be very low and will be indiscernible to the general motorist.

The 95th percentile queues on all approaches, except the through movement on the Charles Street north approach during the PM peak, will increase by no more than 11m, or less than 2 vehicles. For the through movement on Charles Street north during the PM, the queue will increase by approximately 15m, or less than 3 vehicles.

Furthermore, it is noted that the left turn deceleration lane to the western ingress at Cimitiere Street begins approximately 175m west of Charles Street. With 95th percentile queues of less than 150 metres expected on this approach, vehicle entry to this access point will not be obstructed by queued vehicles.

The SIDRA results indicate that the proposal will have no significant impacts on the operation of this intersection, with minimal increases to the Degree of Saturation, queues and delays of movements.

Table 8: SIDRA Intersection Analysis – Cimitiere Street / Charles Street - AM Peak Hour

Approach	Turn	Existing Conditions			Future Conditions		
		D.o.S	95 th ile Queue	Av Delay	D.o.S	95 th ile Queue	Av Delay
Charles Street North	Left	0.112	5.9 m	43.0 sec	0.117	6.1 m	42.2 sec
	Through	0.121	15.7 m	38.3 sec	0.119	15.9 m	37.4 sec
	Right	0.121	15.7 m	46.5 sec	0.119	15.9 m	45.7 sec
Cimitiere Street West	Left	0.178	36.0 m	17.9 sec	0.179	36.6 m	18.3 sec
	Through	0.546	143.4 m	12.4 sec	0.549	145.8 m	12.9 sec
	Right	0.546	143.4 m	21.3 sec	0.549	145.8 m	21.8 sec
Charles Street South	Left	0.411	13.0 m	43.6 sec	0.406	12.8 m	42.7 sec
	Through	0.549	83.8 m	41.2 sec	0.538	83.6 m	40.3 sec
	Right	0.549	83.8 m	49.5 sec	0.538	83.6 m	48.6 sec
Cimitiere Street East	Left	0.209	10.2 m	17.1 sec	0.222	10.9 m	17.5 sec
	Through	0.209	40.3 m	12.4 sec	0.222	43.0 m	13.8 sec
	Right	0.209	40.3 m	20.8 sec	0.222	43.0 m	22.4 sec
Intersection		0.549	143.4 m	20.7 sec	0.549	145.8 m	21.1 sec

Table 9: SIDRA Intersection Analysis – Cimitiere Street / Charles Street - PM Peak Hour

Approach	Turn	Existing Conditions			Future Conditions		
		D.o.S	95 th ile Queue	Av Delay	D.o.S	95 th ile Queue	Av Delay
Charles Street North	Left	0.210	10.9 m	31.0 sec	0.385	20.3 m	30.3 sec
	Through	0.180	28.8 m	26.2 sec	0.259	43.3 m	26.5 sec
	Right	0.180	28.8 m	34.5 sec	0.259	43.3 m	34.7 sec
Cimitiere Street West	Left	0.143	29.7 m	25.9 sec	0.155	32.4 m	27.2 sec
	Through	0.438	98.7 m	22.8 sec	0.476	105.6 m	25.0 sec
	Right	0.438	98.7 m	32.3 sec	0.476	105.6 m	34.8 sec
Charles Street South	Left	0.349	10.9 m	31.0 sec	0.340	10.6 m	29.7 sec
	Through	0.439	81.7 m	28.2 sec	0.475	88.4 m	29.4 sec
	Right	0.439	81.7 m	36.5 sec	0.475	88.4 m	37.7 sec
Cimitiere Street East	Left	0.400	21.0 m	25.5 sec	0.477	25.4 m	26.9 sec
	Through	0.400	90.2 m	23.2 sec	0.477	100.9 m	26.9 sec
	Right	0.400	90.2 m	31.9 sec	0.477	100.9 m	36.3 sec
Intersection		0.439	98.7 m	26.6 sec	0.477	105.6 m	28.7 sec

7.7.3 Charles Street/Canal Street

As the intersection of Charles Street/Canal Street will provide the main access route to the proposal, the future operation of this intersection has been modelled using SIDRA, based on the expected traffic distributions provided within the preceding section.

It is noted that the AAMI Insurance Centre is located adjacent to the site, currently providing 7 on-site parking spaces and there is also some on-street parking provided within Canal Street. Inbound access to the AAMI car park is provided via Canal Street, with vehicles exiting directly onto Charles Street. With regard to the analysis for Charles Street/Canal Street, it has been conservatively assumed that parking within the AAMI site and on-street will generate one vehicle movement per space, split evenly between inbound and outbound movements, therefore generating in the order of 15 vehicle movements to the Charles Street/Canal Street intersection during both peaks.

The results of the SIDRA Intersection analysis are summarised in Table 10.

Table 10: SIDRA Intersection Analysis – Future Conditions

	Approach	Turn	Degree of Saturation	95 th ile Queue	Average Delay
AM Peak	Charles Street N	Through	0.040	1.6 m	0.8 sec
		Right	0.040	1.6 m	9.4 sec
	Canal Street	Left	0.014	0.4 m	9.7 sec
		Right	0.014	0.4 m	10.0 sec
	Charles Street S	Left	0.107	0.0 m	8.3 sec
		Through	0.107	0.0 m	0.0 sec
Intersection			0.107	1.6 m	4.2 sec
PM Peak	Charles Street N	Through	0.046	1.7 m	1.0 sec
		Right	0.046	1.7 m	9.6 sec
	Canal Street	Left	0.179	5.3 m	11.0 sec
		Right	0.179	5.3 m	11.3 sec
	Charles Street	Left	0.128	0.0 m	8.3 sec
		Through	0.128	0.0 m	0.0 sec
Intersection			0.179	5.3 m	5.3 sec

Reference to the above indicates that the Charles Street/Canal Street intersection will operate under 'excellent' conditions during both peaks, with significant capacity available at this intersection.

7.8 Traffic Impact

With consideration of the above, the existing operation of the site sees the traffic generation generally tidal in nature, being inbound during the AM and outbound during the PM peaks and the proposed development includes an on-site car parking provision of approximately 50% of that which is currently approved for the site.

Traffic generated to the intersection of Wellington Street/Bathurst Street/William Street is considered to be low and is expected to be accommodated at this intersection and the SIDRA analysis indicates that the proposal will have no significant impact on the operation of the Cimitiere Street/Charles Street intersection during the peaks and the main access route via Canal Street is expected to operate under 'excellent' conditions.

It is not expected that the proposed development will generate a level of traffic in excess of that which is currently approved for the site, in fact, it is expected that the reduced parking provision will result in a reduction in peak hour vehicle movements generated by the site. The proposed uses will also result in a more evenly spread distribution of traffic inbound and outbound from existing, and therefore there will be less pressure in peak directions than under current conditions.

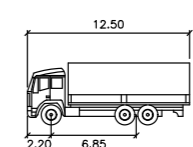
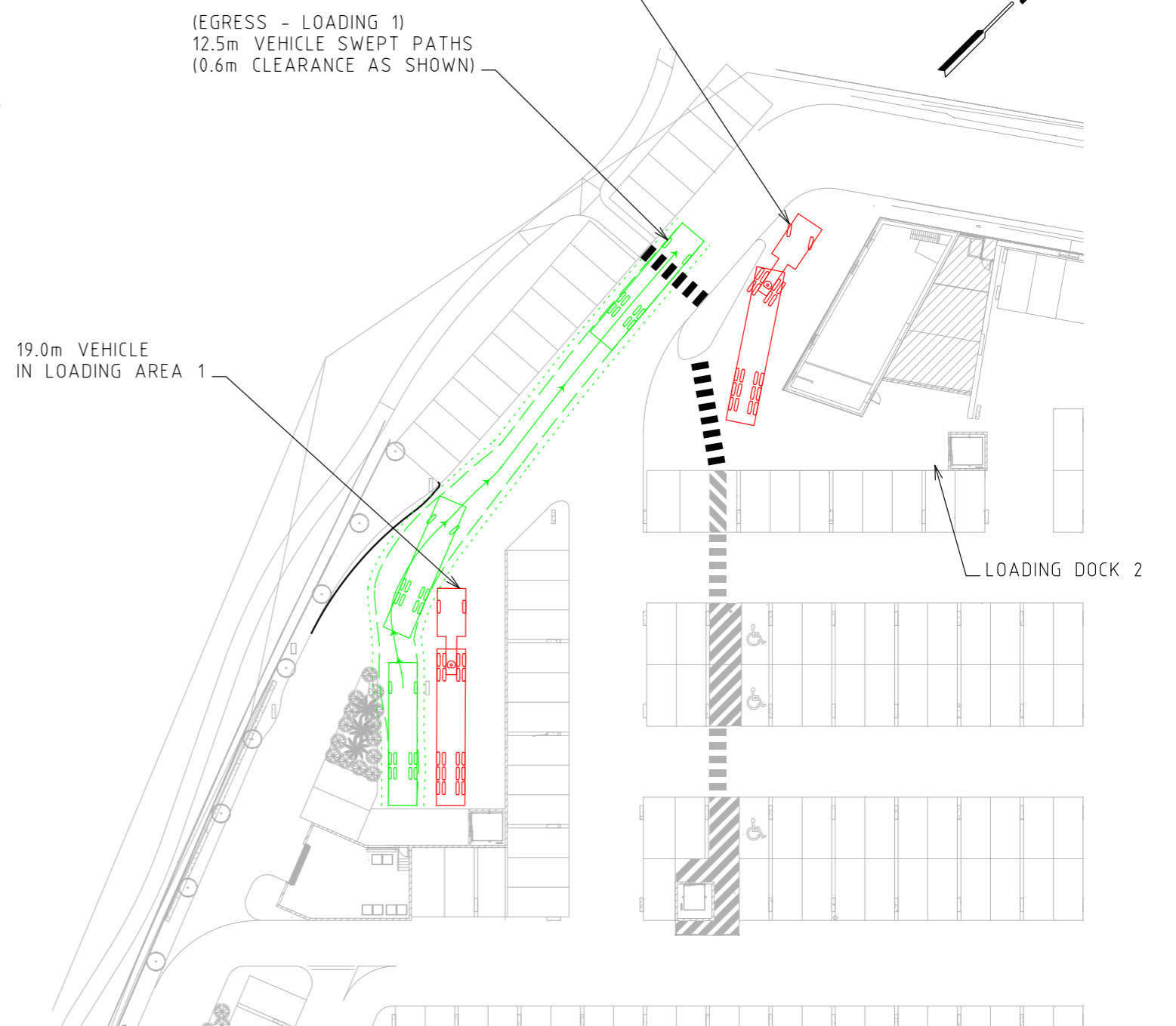
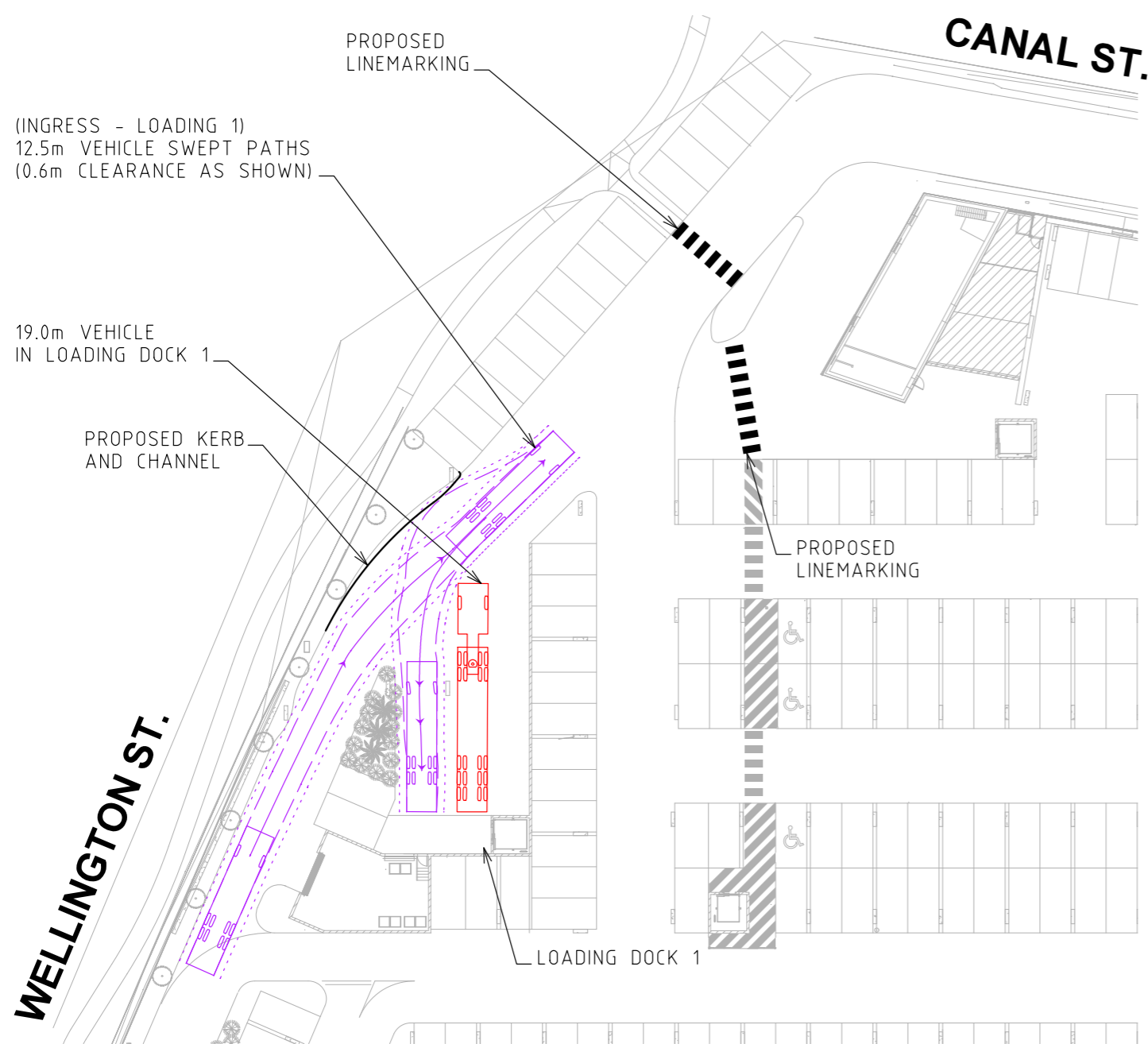
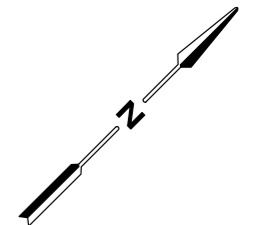
Furthermore, the proposed development is not expected to have a detrimental impact on the operation of the surrounding road network when compared with the existing and approved developments for the site, in fact, with a reduced on-site parking provision, the proposal could be expected to have a reduced impact on the road network from that which is currently approved.

8 Conclusions

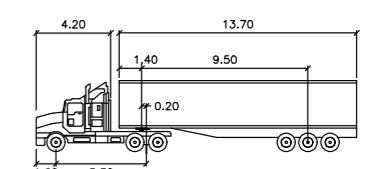
Based on the foregoing analysis it is concluded that;

- The proposed parking provision of 271 spaces represents a shortfall of 377 spaces under the Planning Scheme requirements;
- The reduced provision is supported by the Parking and Sustainable Transport Strategy for the City of Launceston;
- Based on case study data held by Cardno the proposed parking provision is likely to accommodate the peak parking demands generated by the site;
- The proposed development is expected to generate up to 465 and 233 vehicle trips during the Saturday and Friday PM peak periods respectively and traffic generated to the site in the weekday AM peak hour is expected to be generally inbound associated with staff;
- The proposal represents a reduction in the approved parking provision on the site by approximately 50%, which is expected to result in a reduced traffic generation from the currently approved development;
- Traffic generated to the intersection of Wellington Street/Bathurst Street/William Street is considered to be low and is expected to be accommodated at this intersection;
- SIDRA Intersection analysis indicates that the proposal will have no significant impact on the operation of the Cimitiere Street/Charles Street intersection during the peaks; and
- The main access route via Canal Street is expected to operate under 'excellent' conditions during the peak periods.

Annex 1: Loading Swept Paths



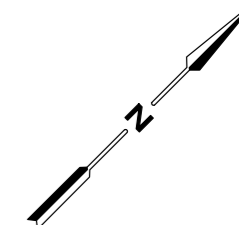
SU TRUCK meters
Width : 12.50
Track : 2.20
Lock to Lock Time : 6.0
Steering Angle : 36.6



S ARTICULATED 19M meters
Tractor Width : 4.20
Tractor Track : 1.40
Tractor Wheelbase : 1.60
Trailer Width : 13.70
Trailer Track : 5.30
Lock to Lock Time : 6.0
Steering Angle : 27.7
Articulating Angle : 70.0

CG107883SK02-A: (LOADING AREA): PROPOSED COMMERCIAL DEVELOPMENT - CNR CHARLES & CIMITIERE STREET - LAUNCESTON, TAS
12.5m VEHICLE SWEEP PATH ANALYSIS (0.6m CLEARANCE AS SHOWN)
SCALE - 1:500 @ A3 DATE - 25-05-2011

CAD FILE: \\DRATRAFF\2007\CG107883\Drawings\CG107883SK02-A.dgn DATE PLOTTED: 26/05/2011 10:23:42 AM



(INGRESS - LOADING 2)
19.0m VEHICLE SWEEP PATHS
(0.6m CLEARANCE AS SHOWN)

(EGRESS - LOADING 2)
19.0m VEHICLE SWEEP PATHS
(0.6m CLEARANCE AS SHOWN)

(INGRESS - LOADING 1)
19.0m VEHICLE SWEEP PATHS
(0.6m CLEARANCE AS SHOWN)

(EGRESS - LOADING 1)
19.0m VEHICLE SWEEP PATHS
(0.6m CLEARANCE AS SHOWN)

PROPOSED LINEMARKING

CANAL ST.

PROPOSED KERB AND CHANNEL

12.5m VEHICLE IN LOADING DOCK 1

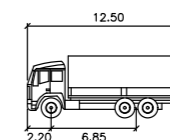
PROPOSED LINEMARKING
LOADING DOCK 2

LOADING DOCK 1

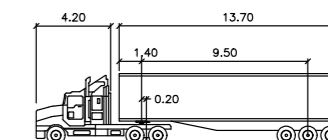
12.5m VEHICLE IN LOADING DOCK 1

LOADING DOCK 2

WELLINGTON ST.



SU TRUCK meters
Width : 12.50
Tractor Width : 2.20
Tractor Track : 6.85
Lock to Lock Time : 6.0
Steering Angle : 36.6



S ARTICULATED 19M meters
Tractor Width : 4.20
Tractor Track : 1.60
Trailer Width : 1.40
Trailer Track : 5.30
Lock to Lock Time : 6.0
Steering Angle : 27.7
Articulating Angle : 70.0

CG107883SK02-B: (LOADING AREA): PROPOSED COMMERCIAL DEVELOPMENT - CNR CHARLES & CIMITIERE STREET - LAUNCESTON, TAS
19.0m VEHICLE SWEEP PATH ANALYSIS (0.6m CLEARANCE AS SHOWN)

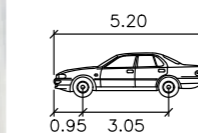
SCALE - 1:500 @ A3 DATE - 25-05-2011

DATE PLOTTED 26/05/2011 10:26:43 AM

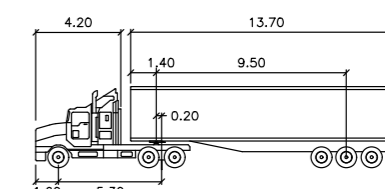
CAD FILE: \\DRATRAFF\2007\CG107883\Drawings\CG107883SK02-B.dgn

Annex 2: Concept Access Plan

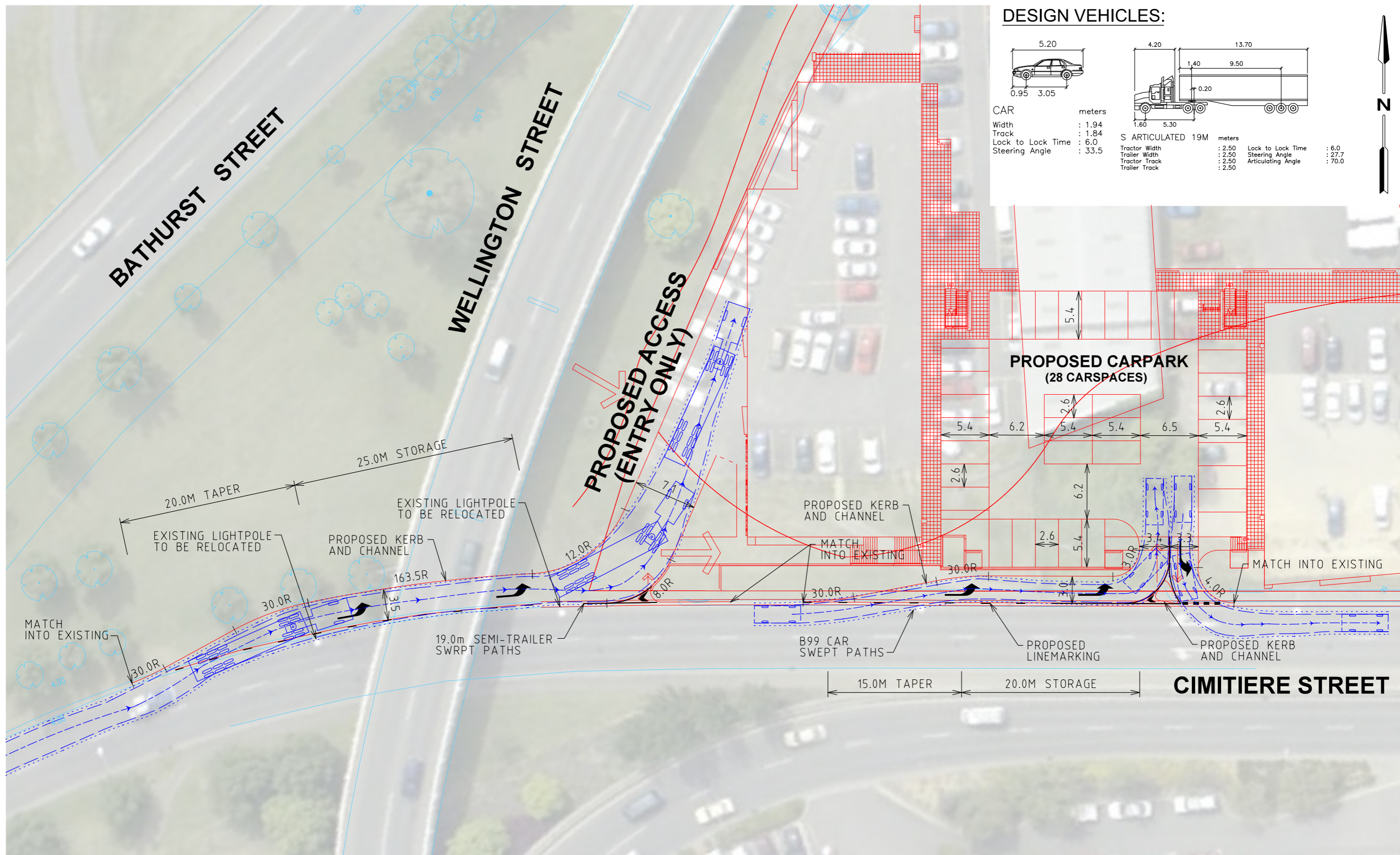
DESIGN VEHICLES:



CAR meters
Width : 1.94
Track : 1.84
Lock to Lock Time : 6.0
Steering Angle : 33.5



S. ARTICULATED 19M meters
Tractor Width : 2.50
Trailer Width : 2.50
Tractor Track : 2.50
Trailer Track : 2.50
Lock to Lock Time : 6.0
Steering Angle : 27.7
Articulating Angle : 70.0



CG107883SK01P3 - PROPOSED COMMERCIAL DEVELOPMENT - CNR CHARLES & CIMITIERE STREETS - LAUNCESTON (TASMANIA)
PROPOSED SITE ACCESS & 19.0m SEMI AND B99 CAR SWEPT PATHS ANALYSIS.

SCALE - 1:400 @ A3 - DATE - 18.07.11

DATE PLOTTED: 08/07/2011 10:29:22 AM

CAD FILE: D:\DRA\TRAFF\2007\CG107883\Drawings\CG107883SK01.dgn

26 June 2007

Clifton Coney Group
Level 2 Buidling B
99 Conventry Street
South Melbourne VIC 3205

Attention: Stuart Moore

Dear Stuart

RE: Preliminary Results of Environmental Site Assessment of CH Smith site, located 16 - 24 Charles St, Launceston TAS

Coffey Environments Pty Ltd (Coffey Environments) is pleased to provide the Clifton Coney Group with the preliminary results of the Environmental Site Assessment (ESA) conducted at the CH Smith site, located at 16 – 24 Charles Street, Launceston (the site).

OBJECTIVES

The objectives of the ESA were to assess whether existing and/or previous activities at the site which were identified during the Phase 1 ESA completed by Coffey Environments in April 2007 have impacted upon soil and groundwater underlying the site; and to evaluate if any identified impact may pose a threat to human health and/or the environment.

The purpose of the Phase 2 Assessment was to:

- Collect soil and groundwater samples at various locations across the site;
- Assess the potential soil contamination which may exist in the fill material and natural soil;
- Assess the potential groundwater contamination which may exist in the groundwater;
- Compare results to recognised ecological and health based guidelines; and
- Interpret the impact of any identified contamination at the site.

SITE LOCALITY DESCRIPTION

Land use within a 500 m radius of the site is a mixture of residential and/or open land.

Sensitive site uses within a 500 m radius of the site include:

- Commercial properties located adjacent and nearby to the site;
- Royal Park located approximately 50 m west of the site; and
- The North Esk River, located approximately 200 m northwest of the site, which flows into the Tamar River 400 m northwest of the site.

Land uses adjacent to the site are summarised below.

- Canal Street lies adjacent to the site's northern boundary, beyond which are commercial properties.
- Directly east of the site is Charles Street, beyond which is the court house and police headquarters.
- Adjacent to the site's southern boundary is Cimitiere Street, with commercial properties located beyond.
- Wellington Street is located on the site's western boundary, with Royal Park beyond.

PREVIOUS ENVIRONMENTAL INVESTIGATION SUMMARY

Coffey Environments conducted a Phase 1 investigation in April 2007. The findings of this investigation are summarised in the Phase 2 report. Coffey Environments are not aware of any other previous environmental investigation at the site.

A preliminary geotechnical investigation was undertaken by Coffey Geotechnics in April 2007. It is to understand that the investigation was for the proposed development of the C H Smith Site (construction of 2 levels of basement, a 4 and a 6 storey buildings). The objective of this investigation was to assess the general subsurface conditions at the site; develop a preliminary geotechnical model of the site; and comment on the types of footings that might be used on the site. The report has made several recommendations for the site including ground improvements.

SCOPE OF WORK

Limited assessment of soil at the site was conducted to assess both current and historic potential contamination. Eight soil bores (SB3 – SB10) were drilled in grid locations across the site to investigate the potential contamination related to fill, identified in the Phase 1 investigation conducted by Coffey Environments (Phase 1 ESA Coffey Environments, April 2007). Two additional soil bore locations were drilled in the area surrounding the down-gradient of all site infrastructures and USTs, SB1 and SB2 respectively.

Field work was conducted by Coffey Environments on 30th to 31st May 2007 and 7th June 2007.

SITE GEOLOGY AND HYDROGEOLOGY

Geology

The subsurface lithology of the site is summarised in the following table.

Site Specific Geology

Depth (mbgs)	Soil Description
0.0 – 0.2	SURFACE COVER: gravel, grass, bitumen or concrete
0.2 – 1.0	FILL: gravely sand or clay and angular and rounded gravels.
0.5 – 4.0	CLAY or Sandy CLAY: dark brown to grey and wet.

Hydrogeology

Current groundwater gauging results are summarised below:

- Static depth to groundwater was encountered between 1.190 mbgs (MW2) and 2.200 mbgs (MW1).
- Based on the groundwater contour plan and the location of the nearest surface water body, the anticipated groundwater flow direction is northwest (towards the River Tamar) at a gradient of approximately 0.750.

SOIL ANALYTICAL RESULTS

Nominated Investigation Criteria

Based on the current and proposed land use of the site (ongoing commercial use), the following Nominated Investigation Criteria are used:

- NEPM (1999) National Environment Protection (Assessment of Site Contamination) Measure (NEPM), threshold concentrations for Commercial/Industrial use; and
- New South Wales Environmental Protection Agency (NSW EPA) (1994) Contaminated Sites Guidelines for Assessing Service Station Sites – “Threshold Concentrations for Sensitive Land Use (Protection of Human Health)”.

Results

Soil analytical results are summarised below.

- All soil samples submitted for analysis contained contaminant concentrations below the laboratory limit of reporting (LOR) or below the nominated investigation criteria with the exception of chromium, copper, nickel and mercury.
- Three sample locations (SB2, SB6 and SB9) contained chromium concentrations above the guidelines for the protection of soil ecosystems. SB2, located adjacent to the Launceston Function Centre had nickel concentrations in excess of the investigation levels (ILs). SB6, located adjacent to the CH Smith Marine building, had copper concentrations in excess of the ILs. SB9, located in the southwest corner of the site, had mercury concentrations in excess of ILs. SB8, located along the southeastern boundary of the site, had nickel concentrations in excess of ILs at a depth of 0.2 mbgs.
- In general, the elevated chromium, nickel, copper and mercury concentrations were associated with fill material (0 – 0.5 mbgs) located across the site, with the exception of chromium and mercury concentrations in SB9, which were associated with natural surface soils (0.2 mbgs).
- The concentrations of chromium, copper and nickel in the fill material were not considered to present a significant risk to soil ecosystems for the following reason.
 - In the event that the whole or part of the site is cleared for growing plants, it is unlikely that the fill material at the site would be used as a growing medium (i.e. it is more likely that imported topsoil would be used).

- Due to the detection of chromium and mercury concentrations in excess of ILs in the southeastern corner of the site (SB9) it is recommended that the top layer of soil is also removed in this area prior to growing plants.
- The results of the soil sampling and analysis program indicated that contaminant concentrations in soil were below the adopted soil quality guidelines for the protection of human health.

GROUNDWATER ANALYTICAL RESULTS

Nominated Investigation Criteria

The assessment criteria adopted for assessing groundwater at the site is based on the protection of all the potential beneficial uses listed above. Nominated investigation levels (ILs) are included with tabulated groundwater analytical results and have been principally derived from the following Australian guidelines.

- NEPM (1999) National Environment Protection (Assessment of Contaminated Sites) Measure.
- ANZECC (1992 and 2000) National Water Quality Management Strategy. Australian and New Zealand Guidelines for Fresh and Marine Water Quality (95% Species Protection).
- NHMRC/ARMCANZ (2004) National Water Quality Management Strategy. Australian Drinking Water Guidelines.

Results

Groundwater analytical results are summarised below.

- All groundwater samples submitted for analysis contained contaminant concentrations below the laboratory limit of reporting (LOR) or below the nominated investigation criteria with the exception of boron, chromium, copper, lead, selenium and zinc in both groundwater samples submitted for analysis.

The significance of metal concentrations (boron, chromium, copper, lead, selenium and zinc) that were above the water quality objectives for ecosystem protection is discussed below:

- The concentrations of metals in the groundwater (as noted above) were considered to be indicative of natural/background concentrations, based on the following.
 - The concentrations of metals in the down hydraulic gradient monitoring wells (MW2), was similar to what was detected in the up hydraulic gradient monitoring well (MW1).
 - Metal contamination in soil follows the same widespread pattern, indicating high background/natural metal levels.

Therefore, metal concentrations in groundwater were not considered likely to have an adverse impact (attributable to the site) on the ecosystem of the likely surface water receiving body (North Esk River) for the following reasons.

- Metal concentrations in groundwater were considered to be naturally occurring and similar to the regional groundwater conditions.
- Generally, the metal concentrations in groundwater were relatively low and exceedences of the water quality objectives were relatively low.

Concentrations of lead and selenium were above the adopted groundwater quality objectives for the protection of primary contact recreation in both wells. Although the protected beneficial use of Primary Contact Recreation has been precluded by metals impact, this beneficial use is considered unlikely to be realised due to the following.

- The current use of the site is as a commercial/industrial premises with no known current plan to abstract or use groundwater.
- There are no registered bores within a 500m radius of the site. It is not compulsory for groundwater bores to be registered in Tasmania however the low yield of the aquifer and availability of reticulated water means that it would be unlikely that groundwater would be abstracted or used in the future.
- In the event that North Esk River was used as a recreational bathing water body, based on groundwater contaminant levels found across the site and the distance to the river, immediate adverse impacts to Primary Contact Recreation off site are unlikely.

The beneficial uses of Agriculture, Parks and Gardens and Stock Watering were precluded by selenium. However, based on the zoning of the site and the availability of a reticulated water supply it is considered unlikely that the potential beneficial uses of Agriculture, Parks and Gardens and Stock Watering will be realised.

SUMMARY WITH RESPECT TO OBJECTIVES

The results of the ESA indicate the following:

- All soil samples submitted for analysis contained contaminant concentrations below the laboratory limit of reporting (LOR) or below the nominated investigation criteria with the exception of chromium, copper, nickel and mercury.
- In general, the elevated chromium, nickel, copper and mercury concentrations were associated with fill material (0 – 0.5 mbgs) located across the site, with the exception of chromium and mercury concentrations in SB9, which were associated with natural surface soils (0.2 mbgs).
- All groundwater samples submitted for analysis contained contaminant concentrations below the laboratory limit of reporting (LOR) or below the nominated investigation criteria with the exception of boron, chromium, copper, lead, selenium and zinc in both groundwater samples submitted for analysis.
- Metal concentrations in groundwater were not considered likely to have an adverse impact (attributable to the site) on the ecosystem of the likely surface water receiving body (North Esk River) given that, metal concentrations in groundwater were considered to be naturally occurring and similar to the regional groundwater conditions.
- Lead and selenium concentrations were above the relevant water quality criteria for the Primary Contact Recreation beneficial use. The beneficial use of primary contact recreation is unlikely to be realised on site given the availability and low cost of reticulated water. There is the potential for the beneficial use of Primary Contact Recreation to be realised off site after discharging to the North Esk River located 200 m northeast of the site. However based on groundwater contaminant levels found across the site and the distance to the river, immediate adverse impacts to Primary Contact Recreation off site are unlikely.
- The beneficial uses of Agriculture, Parks and Gardens and Stock Watering were precluded by selenium contamination. However, based on the zoning of the site and the availability of a reticulated water supply it is considered unlikely that the potential beneficial uses of Agriculture, Parks and Gardens and Stock Watering will be realised.

Based on the concentrations of contaminants reported in both soil and groundwater during this investigation, the risk to the human health of commercial users of the site and the environment is considered to be low. To minimise any potential risk, Coffey Environments recommends the removal of the first 0.5 m of either the fill material or natural soils (in the southeastern corner) in areas where plants are to be grown.

On comparison of the results to the Department of Tourism Arts and Environment (DTAE) Information Bulletin 105 *Classification and Management of Contaminated Soil for Disposal* (August 2005), it was found that metals contamination detected in several locations across the site is in excess of Level 1 Fill Material. If the first 0.5 mbgs of soil was to be removed for construction or landscaping purposes and disposed of off-site it would likely be classified as Level 2 Low Level contaminated soils and require DTAE approval prior to disposal.

For and on behalf of Coffey Environments Pty Ltd



Aryel Pylotis
Project Supervisor



Anne Hellie
State Manager TAS



2 December 2014

Launceston City Council
GPO Box 396
LAUNCESTON TAS 7250

To whom it may concern

HERITAGE IMPACT STATEMENT - CH SMITH SITE, LAUNCESTON

Ireneinc Planning has been engaged by Brile Pty Ltd to prepare and lodge a development application on their behalf in relation to land at 9 Canal Street and 16-24 Charles Street. Our client is seeking to amend their proposal as approved under planning permit DA0343/2011.

The Heritage Impact Statement which was completed by Paul Davis in June 2011 and that formed part of the application for Permit DA0343/2011 is submitted as part of this application. As some of the heritage works have been completed and as the current application includes some differences in comparison to the previous proposal a list of clarifications is provided below. It is recommended that these clarifications are read in conjunction with the HIS.

- The archaeological works as detailed in the HIS and by condition to the existing approval have been completed and signed off by the THC.
- The CH Smith Marine Building has been demolished.
- The rear of the CH Smith Wool Store warehouse is proposed to be retained as part of the current application unlike the existing approval for demolition.
- The Victorian warehouse/conference centre has been partially demolished and the front façade wall retained as described in the HIS.
- The roof of the CH Smith Office Building which was proposed to be demolished as part of the existing approval will be retained as part of the current application.
- The works to the former Cordial Factory as detailed in the HIS does not form part of the current application.
- The remaining sections and internal works to heritage buildings as described are proposed to be carried out and form part of the current application.

Should you have any queries in relation to any of the above please contact me on 6234 9281.

Yours sincerely

Sara Vikstrom
Planner

IRENEINC PLANNING

smithstreetstudio

ireneinc

49 Tasma St, North Hobart, TAS 7000

Tel (03) 6234 9281

Fax (03) 6231 4727

Mob 0418 346 283

Email planning@ireneinc.com.au

CH Smith Site

Charles St Launceston

Heritage Impact Assessment

for

Five Zero Holdings

June 2011

Introduction

A development application has been prepared to construct a new retail centre with parking on what is generally known as the CH Smith site in Charles Street Launceston. The development proposes both new construction and the retention, conservation, adaptation and incorporation of the identified heritage buildings into the overall development. This HIS supports the development proposal and is provided as required under both the Launceston Planning Scheme and the Tasmanian Cultural Heritage Act to accompany the two separate applications for development consent and works approval.

An important aspect of the process undertaken in submitting this application is that both Launceston City Council and Heritage Tasmania have been consulted and invited to make comment on the draft application documents and to provide any advice in submitting the applications. While it is clear that preliminary advice is 'advisory' and not a form of approval, the process has allowed a more responsive submission to be made that has addressed many of the matters raised in the preliminary advice.

The CH Smith site has had a long and often not impressive history, particularly in more recent years. Significant damage was done to the site after an attempt to demolish the buildings leaving a number of them in a damaged and deteriorated state that has resulted in significant deterioration of fabric. One of these buildings is in such an advanced state of deterioration that it is no longer capable of recovery.

More recently a DA was granted, after an appeal, for an ambitious development of the site. This did not eventuate and the consent lapsed. As part of that process, the Tribunal made a decision on the heritage values of the place - reflected in which parts of the buildings had to remain and which could be removed or altered - and most recently that has been reflected by the THC in a revised (and agreed) re-registration of the site. This registration forms the basis of the current proposal and with only a few exceptions the current proposal carefully reflects the listing.

The current owner has commissioned both the design of the proposed bulky goods centre (the zoned use for the site) and the heritage works at the same time. This has had the benefit of being able to incorporate heritage requirements into the proposal from the early design development. There has been substantial design development to achieve better heritage outcomes while retaining the viability and build ability of the proposal.

One of the key difficulties of the site with its zoning is that the requirements of bulky goods retailing and the fabric of the heritage buildings are not highly compatible. This has required careful integration of the elements and has determined the design outcome in many respects.

A key issue that arose during earlier applications and the court determination was the archaeology of the site. An initial assessment was prepared and has formed the basis of this submission. After extensive discussions with the THC related to site archaeology, a further report has been prepared and included in this application that sets out a methodology to approach the archaeological resource of the site. This is addressed later in the HIS.

This HIS considers the current proposal and its benefit as well as potential impacts on site significance. It assesses this in terms of the approved CMP for the site, the THC listing data sheets (which were developed from the CMP), the archaeological assessment and the potential to recover not only fabric but also meaningful relationships between parts of the site and to allow the public to appreciate and interpret the history of the site in the future.

It is inevitable, for a site such as this, where there has been significant deterioration and loss of fabric, that heritage values will be impacted by development. While the project has seriously attempted to minimise those impacts, it is also very clear that if some form of viable development does not take place that much of the fragile heritage fabric will be lost in the near future. A large part of the significant fabric is at very high threat of loss and action to recover these buildings and elements is essential. This proposal will allow that to happen.

A benefit of the proposal is the integration of the heritage buildings into the core of the design. The heritage buildings will have to be conserved and adapted as part of the core building works for the centre to operate. This provides a high level of certainty to both council and the heritage council that the heritage works will be undertaken as part of the core construction work.

Another key aspect of the proposal is the re-use of material that is to be removed from various parts of the site. Where brickwork from earlier parts of buildings is to be removed it will be re-used to undertake repairs, reinstate missing elements and if sufficient material is available as part of the public domain. The main public entry to the complex, on the corner of Charles and Cimitierre Streets will use recovered timber from the wool store, brickwork from various parts of the site and other elements as they are recovered.

We also note that at the rear of the Canal Street warehouse that we are retaining the end wall of the former large warehouse (this is not part of the THC listing and is not required to be retained) which allows us to use the interstitial space between the 1840 Canal St building and the current convention centre. This is a positive addition to the heritage of the site.

Unfortunately the condition of no 22 Charles St, the former Fry residence, has so deteriorated that the interior of the building is collapsing and cannot be recovered. Consequently we propose to retain the joining wall to no 24, the front facade with its joinery, if possible the exposed sidewall, but remove all other fabric. Remnant joinery will be recovered and materials will be salvaged for re-use. In recognition of the importance of the building and its setting, we propose to reconstruct it (with a new rear addition), retain the current floor levels, rebuild the original roof form and detail, and integrate the building with no 24 as a single tenancy. This allows us to use the

reconstructed no 22 to accommodate any service areas, vertical access etc and to leave no 24 without any fabric impacts beyond those essential to recover the building. We also note that we propose to remove the rear bay of no 24. This area has had severe exposure and deterioration and cannot be recovered. The roof form will be adjusted with the hipped form being shortened by a bay.

The final change of note is to the upper floor of the corner building. All three floors are retained in the proposal and a partial new upper floor is added over a section of the building. This requires cutting back a section of the earlier roof located behind the projecting parapet.

The Site

The site is a whole city block bounded by Cimitierre, Charles, Canal and Wellington Streets. The heritage buildings to be retained are located along Charles and Canal Streets. The site has been partially excavated with approximately two levels below the entry point in Charles Street. A number of structures are to be demolished. They include:

- The present CH Smith Marine building,
- The rear of the Charles St woolstore with its rear addition
- The recent raised upper floor of the CH Smith office building
- The conference centre apart from the street and end wall
- Sections of other buildings as separately described.

The demolition of the non-heritage listed elements is in accordance with the previous tribunal consent and accords with the current heritage listing.

A large portion of the site is affected by archaeological zoning as set out in the separate archaeological report. This is addressed separately.

Heritage Listing

The THC data sheet for the site is appended at the end of this statement. The boundary of the listing is set out below for clarity. This assessment only refers to elements and areas within the listing boundary, apart from noting what parts of the site are to be demolished.

The significance of the place now resides in:

- The identified heritage buildings
- The physical fabric of those buildings
- The streetscape grouping and presence of the facades within Charles and Canal Streets especially
- The relationship of the buildings to each other and other heritage buildings in the locality
- The ability to see the group from a range of locations, in particular longer views within Charles Street
- The potential archaeological resource of the site
- The identifiable links to the CH Smith name and brand

This significance is adversely impacted by the condition of the buildings and the ongoing loss of fabric through deterioration.

The Conservation Management Plan

The CMP was prepared in anticipation of an appeal regarding the approval of an earlier scheme. The CMP was accepted by the RMPAT as an appropriate document to guide the site's development. The HIS responds to the various policies within that document.

Generally the proposal seeks to:

- Retain as much original and early fabric as possible
- Find compatible uses for the heritage buildings
- Integrate the heritage buildings into the development in a meaningful way
- Limit service interventions into heritage buildings
- Use the character and materials of the heritage buildings to guide their internal fitout and use
- Reinststate the streetscape form, detail, finishes and colours of the heritage buildings
- Develop sensitive connections to the new fabric
- Reveal and expose the heritage buildings and fabric wherever possible
- Investigate the archaeological potential of the site.

The Proposal

The proposal is set out in the design drawings and the separate heritage drawings that form the application. We have intentionally separated the documentation to provide as clear a guide as we can to what is taking place to each of the listed buildings. The drawings provided are:

- 1 Measured drawings as the site currently exists.
- 2 Demolition drawings
- 3 Drawings of each building showing the extent of conservation and adaptive re-use works.

A schedule of works to each building is also provided.

Each of the heritage buildings has nominated uses, either as part of a larger tenancy (top floor of the corner building), or as separate smaller tenancies. Each of these buildings falls below the minimum floor area requirements of the zone for a tenancy (500 square metres), consequently a heritage concession is required to allow the buildings to accommodate uses that are suitable to their scale and layout without having to impact on their heritage values by incorporation into larger tenancies. We have attempted to nominate uses that best fit the site in general and the specific heritage buildings in particular.

In detail and in relation to each listed building the proposal is:

24 Charles Street

Description

The building, constructed as a townhouse in c1850 is a three level building with a rear stair. The ground floor has been converted to commercial use at the front with a concrete side wall and concrete floors added. There is a rear room. The first floor has two now linked spaces, each with the remains of a fireplace and the second level has two major rooms, one divided with a timber partition of later date than the original construction.

The front facade retains much of its joinery, although now badly damaged, to the upper floors and a mid twentieth century shopfront. The rear wall has had joinery replaced with twentieth century elements some in new openings.

The roof was removed in the 1970 period and the replacement flat roof has failed and collapsed. Most of the interior has been removed with remnant elements of joinery and wallpapers. The building has been used as a squat and most of the interior appears to have been burnt in the now partially demolished fireplaces.

The stair has partially failed. There has been significant lateral movement in the building with the southern sidewall moving out. There is substantial cracking through the building.

The building has major movement and failure, and is not capable of repair.

Proposed Works

The work involves retention and propping of the front and two sidewalls, and removal of the balance of the building.

Remnant elements of joinery etc will be recovered for sampling. Demolished brickwork will be recovered for re-use.

It is intended to retain the free sidewall, however, with further structural input, it may be necessary to dismantle at least part of the wall. If this is required the wall be reconstructed to its current form using the existing bricks.

The building will be reconstructed within the retained walls, retaining the current floor levels. The new construction will be concrete and will support the facades. An addition to the rear in steel and glass will house a new stair that will serve both Nos 24 and 22. The roof will be reconstructed to its original form and appearance and be clad in unfinished corrugated iron.

Joinery will be reinstated to the facade and the earlier shopfront, known from council drawings will be reinstated into the ground floor.

The building will not be separated into rooms but accommodate a new use with either clear floor plates or new sub-division to accommodate service areas.

A new researched colour scheme will be applied to the exterior of the building.

Variations to CMP and/or THC listing

The CMP seeks to retain and recover the building. At the time of writing the CMP, this was still feasible. Since that time the building has deteriorated significantly to a point where recovery is extremely difficult and somewhat pointless given the loss of most significant material.

Generally the whole site requires extensive conservation work and there will be adequate but inevitably limited funds to undertake it. It is submitted that reconstruction of this building is a sound action, a good use of resources and allows other achievable conservation work to be undertaken elsewhere on the site.

22 Charles Street

Description

Constructed in the 1860's this largely intact warehouse building of three levels, with a small low-height basement, is sandwiched between nos 24 and 20 Charles Street, both of which are likely to provide structural support for the building.

The building had some change over time with minor internal stairs being added, concrete slabs being poured over the timber flooring, changes to the ground floor facade, addition of partitions and various cuts out to the structural frame and subsequent repairs and propping. At some point in its history the front roof was changed from hipped to gabled to allow hoisting of materials from the street, this presumably took place after the adjacent woolstore was built which was constructed across the former loading dock.

With the partial demolition of the rear additions, the rear wall and small parts of the sidewalls collapsed leaving the timber structure floating without support. Temporary bracing tenuously supports the rear of the building.

Most of the roof cladding has been lost and later tarpaulins have failed leaving the structure exposed to water and weather. All gutters and rainwater systems have failed.

Internally the building has painted brick walls and an exposed structure, the front facade is painted render finish.

Some of the original joinery survives, some is damaged but sufficient remains to undertake authentic repairs and replacement. Remnants of the original loading bay doors remain.

Proposed Works

Overall the building is to be retained and conserved with the following changes to the fabric:

- Removal of the badly deteriorated rear bay, cutting back the structure to the first support column.
- Cutting back the rear roof to reconstruct the hipped roof form to the new building footprint.
- Remove the front gable roof form and reinstate the original hipped roof form
- Remove the applied internal fitout of concrete slabs, stairs, remnant partitions etc.

The conservation works proposed are:

- Reinstall missing and damaged joinery throughout
- Repair damaged floors throughout, retaining timber floors, infilling later openings
- Repairing the structure and propping and splicing new elements as required
- Reinstalling the loading dock doors to each level

- Providing a new corrugated iron clad roof with new rainwater systems
- Cleaning back and repainting of internal brickwork and joinery
- Reinstating the early symmetrical ground floor facade with central doorway flanked by windows.

The new works proposed are:

- Minor new openings to connect to the new access stair at no 24
- Provision of new rear wall of glazing and solid panels to front the new arcade area.
- New door and stair on level 2 to provide egress and connect to the new commercial development.

Variations from the CMP and/or THC listing

The CMP seeks to retain this building in its planned form. The only variation of consequence from that overarching policy is the removal of the rear bay. This action is only proposed due to the deteriorated condition of the timberwork in the exposed rear bay.

20 Charles Street - Woolstore

Description

The c1938 wool store is an amalgam of a well designed and executed facade by a prominent Tasmanian architect and a relatively standard warehouse form behind. Generally the Charles Street facade is in good condition with limited change.

The THC listing requires the facade of the building to be retained.

The facade is concrete framed with infill brick panels and a decorative parapet. It is symmetrical with two vehicle openings and flanking windows. The facade is unpainted and retains remnants of the CH Smith sign.

Proposed Works

The proposed works are:

- Demolition of the warehouse behind the facade
- Retention of the facade, the northern return and the framing along the wall of no 22.
- Reinstating original window forms to the window openings and providing new shopfronts to the former door openings
- Retaining the extant timber sliding doors

Variations to CMP and/or THC listing

The work is consistent with the CMP policies and the THC listing.

16-18 Charles and Canal Streets

Description

This much worked over building now presents as a mixture of the 1919 street presentation and the 1830s commercial building behind. In the 1970s the end bay of

the building was demolished and the current driveway and raised upper floor constructed. At the time new stairs and entry were also added.

It is proposed to remove the upper addition and the entry stairs and amenities area, leaving the earlier building on the corner.

The building is painted rendered brick and painted brick externally and a mix of painted brickwork and plaster finishes internally. Nearly all of the joinery has been replaced with aluminium framed joinery, new ceilings and internal linings have been applied and the building has had various service overlays. However, the overall form and presentation of the corner of the building remains with its c 1919 appearance.

The building connects to a rear warehouse, this is addressed below.

Proposed Works

The proposed works are in detail:

Demolition

- Demolish the adjoining buildings except for the Canal Street wall and its' gabled return into the site which is retained.
- Remove the interior fitout throughout the building including ceilings and later linings
- Remove existing services from the building
- Remove part of the main roof as indicated on the drawings for the addition of a new upper level
- Remove sections of the rear wall on the upper level to allow interconnection to the rear new tenancy

Proposed Work

- Infill openings to the rear wall on two levels as detailed
- Construct a new end wall to the stair and arcade area as detailed
- Provide new base fitout of electrical, fire and mechanical
- Provide a new ceiling under the new floor to the top level of the building.
- Refit the building for new commercial and retail uses.

Variations to CMP and/or THC listing

This building was identified as having lesser heritage value than the more intact earlier buildings and was approved for partial removal in the earlier approved development application.

This proposal retains the building (that is the earlier section of the building in contrast to the 1980 additions) with a change to part of the roof to allow a new floor to extend over a section of the building. Apart from this and some interconnection to the new development the work achieves a greater level of retention of heritage fabric than was anticipated in the CMP and satisfies the THC listing that seeks to retain the building in situ without major change.

Canal Street Wall

Description

The Canal Street warehouse is a large clear span structure with a simple gabled roof. The THC listing includes the street facade only of the building. The proposal is to retain this wall and the end wall as it returns behind the Canal Street warehouse and to remove the balance of the building. There will be brickwork and timberwork from the roof trusses and framing to recover to incorporate into the main development.

The Canal Street wall is painted brick with three openings, one infilled, the other openings have contemporary doors and awnings.

Proposed Works

The proposed works, after demolition are:

- Retain one opening with new access as detailed.
- Infill the remaining openings as detailed.
- Remove projecting hoods and detailing applied in recent years.

Variations to CMP and/or THC listing

The CMP and THC listing require the wall fronting the street to be retained, this is achieved.

The retention of the sidewall is in addition to the CMP and THC requirements.

Canal St Warehouse

Description

The c1830s warehouse in Canal Street remains largely intact but in poor condition. An irregular shaped building, built to the lot boundaries, it is two storey of brick construction with a hipped slate (originally) clad roof. It has a timber floor and stair to the upper level and a concrete floor to the ground floor. It has openings to the frontage including a partially blocked loading bay and sets of matching window openings with steel bars and remains of shutters and three openings to the rear for access. A timber partition divides the lower floor.

The building is located on the edge of the original shoreline and had small inlets on either side, founded below water level the structure has extensive rising damp problems exacerbated by rendering the lower sections of walling. The building has also undergone movement with cracking throughout.

The roof is of unusual construction with a perimeter tie beam set inside the outer wall and pegged jack-rafters supporting the lightly framed roof.

The building has an infill to the rear between it and the adjoining warehouse of sawtooth roof construction with southlights.

Proposed Works

The proposal is to retain the building largely in its built form undertaking conservation works to stabilise it, repair work to make it usable and in particular to repair the brick walls. Minimal services are to be added to allow the building a commercial use. Access to the upper floor will be through a new stair in the rear annex area, toilets and kitchen will also be added in this zone outside the core building.

The work in more detail is:

- Remove the current roof cladding, repair the roof structure and fix a new corrugated iron roof over insulation and a light ceiling lining such as thin plywood.
- Install new rainwater system to detail
- Repair the upper floor and floor structure, cutting in new boards to match where required, the centre of the building may require further support and replacement of the shearhead below.
- Retain the stair but do not use.
- Investigate removal of the ground slabs, if retained separate from edge of the building by cutting and removing approx 200mm of concrete to expose the walls.
- Remove fit out and rubbish
- Remove infill to loading dock on ground floor, insert new window to detail within existing timber framing
- Repair existing window openings and insert glazing to detail
- Insert a new window in the upper level loading bay, retain timber doors
- Provide new rear entry doors to existing openings
- Provide new electrical supply and base lighting and power to future detailing. Organise reticulation through cable trays and risers to be designed as part of fitout.
- Repair brickwork by removing sections of failed brickwork and inserting matching bricks recovered from the site. This will need to be done in sections to retain structural integrity. Mortar is to be lime mortar.
- Perimeter drainage is to be provided with slot drains around the building. Cut away pavements and fill with gravel to detail to allow the building to be separated from hard pavements.
- Strip the paintwork from the building and finish with breathable paint system
- Replace damaged stone sills with new stonework.
- Fill cracks with expandable filler to detail prior to finishing.

Variations to CMP

The work accords strictly with the requirements of the CMP and the THC listing requirements.

Process

The design has been developed with significant input from the heritage consultant. This has involved a series of design review meetings, the detailed design of public areas, junctions with new and old, fitting out of the heritage buildings, preparing schedules of works for the heritage buildings etc.

Several meetings have taken place between the heritage consultant and Heritage Tasmania to obtain feedback and address detailed issues. These have resulted in

design changes, clarifications and refinement of the project to address heritage issues.

Response to Launceston City Council Comments

The following points were set out in a response letter to the preliminary plans from Launceston Council:

- 1 *Read the existing heritage buildings as separate buildings and not heritage facades. Consider setbacks and joins. Consider Charles Street view with the large overhanging roof behind the facades.*

This has been achieved by setting the main roof further back from Charles Street which will allow the heritage buildings to be seen in profile and as three-dimensional objects.

The entry area has been re-designed to further setback signage and screening to reveal the heritage buildings when viewed from Charles Street.

The design completely separates new and old buildings structurally with new construction abutting but not being built onto heritage buildings (except where there is a level addition to 16 Charles St). This allows significantly more heritage fabric to be retained (refer to detailed heritage drawings)

Setbacks have been established where new structure abuts old structure so that the separate elements can be read separately.

Additional elements are retained such as the return wall in Canal Street that will give greater legibility to the heritage elements.

Recovered materials are to be used as part of the design of the public areas to feature heritage fabric.

- 2 *Sensitive treatment of heritage buildings in relation to new elements*

As far as possible, given the scale of the buildings and the zoning of the site, the heritage buildings have been treated sensitively.

- 3 *Maximise retention of heritage fabric.*

This has been achieved. There is a high level of retention of fabric where it is possible. It is noted that some elements have deteriorated to a point where they cannot be recovered and some elements are to be removed as previously agreed through the RMPAT process. Overall there is a high level of retention of fabric.

- 4 *Signage*

Signage has been adjusted around the heritage buildings to reduce its scale and location and to set it further back so as not to conflict with the heritage structures. There is signage generally around the site but within Charles and Canal Streets in particular it has been reduced and minimised.

Response to Heritage Tasmania Review

A formal response to the initial design was made by Heritage Tasmania raising a number of matters. Each has been addressed in the revised design submitted as the Development Application.

The key points of that review and how they have been addressed are:

1 *HIS - response to heritage values*

The submission responds to the heritage values of the site through this document and the drawings, the policies and statements as well as the Burra Charter and Heritage Act requirements have been considered and implemented.

There are some variations to the CMP and THC listing, these are set out and discussed. Changes are principally proposed due to the very poor condition of elements and the difficulty of recovery, particularly within cost effective approaches and balancing the overall heritage objectives of the site.

2 *Rationale for demolition of 24 Charles St prudent and feasible alternatives, extant recording, salvage of materials*

The partial demolition of no 24 Charles St is only proposed due to the failed condition of the structure and the relatively low remaining heritage value of the fabric proposed to be removed. It may be technically possible to keep the structure standing and to slowly repair and replace elements, but the net effect of this action will be little different to removing the failed elements and reconstructing them as those elements have little inherent heritage value. This arises as the building has been stripped out with only base fabric remaining.

The remaining elements of greatest value, that is the facades and joinery within them, is all to be retained in situ and recovered.

At this point of the life of this building its value is now found in its streetscape contribution as a building, not just as a facade, that is its three dimensional form. This is even though it is without its roof. These elements, with the reconstruction of the roof are to be retained and the building will continue to read as part of the group and as a complete building.

The prudent and feasible argument is that retaining the sections of building as proposed and reconstructing missing and severely damaged elements is an appropriate response to the heritage values and condition of the place. It is cost effective, responsible and allows funding to be allocated to other equally important and urgent heritage structures on the site. Further it allows additional heritage elements to be retained and adapted and conserved.

An extant recording will be undertaken.

Materials will be salvaged for re-use and repair of other heritage structures.

3 *Interpretation*

It is proposed to undertake interpretation throughout the site. This will developed in detail but in outline will comprise:

- The re-use of elements with explanation and links to the history of the site
- Use of historic images and maps to tell the story of the site displayed throughout the centre
- Display panels orienting visitors to the history of the site and the remaining elements
- Use of archaeological material (to be determined when work takes place) as part of the display throughout the site.

- Material within the heritage buildings telling aspects of the history or stories about each use and building and the key people involved in the site (such as CH Smith).

4 *Staging*

The work is not to be staged.

5 *Archaeological Method Statement*

This forms part of the DA submission.

6 *Detail design of each building, schedules of work, etc.*

These are provided with the DA submission as requested. We have prepared a separate set of heritage drawings with details of demolition, new work, adaptation, conservation, etc with preliminary schedules of work. We have not at this stage detailed all of the services or every repair, we suggest that as we complete the documentation that the final documents be submitted for final review and approval by Heritage Tasmania to address the final stage of documenting.

7 *Accuracy of drawings*

The heritage drawings and revised DA set reflect the existing facades and are based on measured drawings undertaken of the site.

8 *Indicative construction details*

These are provided with the application and address connections between buildings, alignment of new and existing walls (Canal Street) and other details. We have detailed to maximise the retention of heritage fabric.

9 *Timeframe*

As noted earlier, the heritage buildings are integral to the development and cannot be easily separated. The work set out on the drawings will be undertaken within the main construction timeframe and be ready for letting and occupancy when the centre is occupied. The prominent location of most of the buildings requires them to be tenanted for the centre to function.

10 *Material and colour board*

Materials and colours are provided.

11 *Setback of Canal St new work.*

This is incorporated into the drawings and the openings are adjusted as suggested.

12 *Cut back Charles St roof of new building.*

This has been done to align with the already stepped roof section achieving the setbacks recommended. It is noted that it is not feasible to retain sections of the sawtooth roof (even though this would be desirable) as the roof structure runs from the street and has to be removed to construct the new buildings.

13 *Signage to Charles Street*

The panel signs are reduced in size as suggested.

The infill sign is to be reduced to the format of the panel signs as also suggested.

14 Articulating the wall panels

A patterning of the facades is proposed, based on the module of the panels to remove the flat panel appearance. While modest this goes some way to provide patination of the facades.

15 Design of area around Canal St warehouse.

This is the loading area for the site and is difficult to modify in terms of design. The building is to be protected using bollards and pavement treatments can be modified, however the access to the parking and loading is fixed.

16 Design of entry from Charles/Cimitierre corner

The design has been modified to set back the signs, link them to an entry structure that is separate from the heritage building and to add a lower level shelter for outdoor dining. The whole area has been re-designed, incorporating re-used materials, to create a more refined urban space, accommodating changes of level and avoiding the key archaeological areas.

Response to CMP Policy

The 2003 CMP set out a range of policies for the future management of the site. These are set out below where they are relevant to this proposal. Items in the policy that are 'struck through' are not either uses that are taken up (noting that there were a range of uses for each building set out) or guidelines that have not been adopted. This method of presentation has been used to indicate how the proposal relates to the policy of the CMP. An explanation for any departures from the guidelines is found in the comment column:

Future Use

Address	Potential Use from CMP	Guidelines	Comment
24 Charles St	Retail Commercial Residence	Retain room layout Retain stair Recover interior form of building Retain or reinstate shopfront Reinstate original roof form	The building has deteriorated to a point where it requires rebuilding. This is proposed within the three front and sidewalls. The stair has collapsed and cannot be recovered. A new stair that provides access to both 22 and 24 is proposed to the rear. The shopfront is being retained. The original roof form is being rebuilt. Floors are being reconstructed at their original levels, however the building will be used to accommodate kitchen/toilets/services etc to remove impacts on the intact no 22. Some of the form of the building will be recovered but overall the building interior will not be sub-divided into spaces.

Address	Potential Use from CMP	Guidelines	Comment
22 Charles St	Commercial Retail on ground Gallery cultural uses	Retain open form of floors Reinstate stair into current opening Reconstruct missing side and rear walls, complete form of building Do not replicate original fabric Retain slot opening to north Recover facade to original form	The rear bay is proposed to be removed as it has deteriorated significantly. The roof form will be recovered. A new rear wall of modern design will be provided The floors will be retained as open floors. The facade is to be fully recovered. The stair is not required and the openings in the floor will be infilled showing evidence of the fill. The slot opening is retained. The likely use is as part of a restaurant with no 24 or possibly commercial or gallery use.
Canal St Warehouse	Retail Commercial Restaurant	Retain open form of floors Retain significant finishes and elements Recover front opening and reinstate matching joinery Repair damaged fabric Locate new stair in annex Remove annex structure, retain east wall and reconstruct new infill Do not add drainage and plumbing into original building.	The building will be conserved in its current form, without intrusive services, reinstating the front opening, repairing damaged fabric with new stair and services in the annex area. The east wall of the annex is being retained.

Policies arising

Policy	Response
1 Determine new uses from the table	Satisfied
2 Assess uses for impact	This has been done and impacts are minimised in the selected uses.
3 Provide uses with some public access.	This is achieved
4 Uses for balance of site	Uses are complying with the Planning Scheme
5 Select uses that link site to city	This is achieved in retail uses.
6 Link three heritage sites to broader site uses so they are not marginalised or unused.	Each building is linked either to the main arcade entry area or for the rear warehouse as the proposed management offices for the site with separate access from Canal Street. This building also has potential for separate commercial leasing.

Future Site Development

This section considers the broader development of the site in relation both the buildings and heritage values of the site but also the context of Launceston.

The proposed use and type of development is compliant with the planning scheme provisions in that the site is zoned for bulk goods retail with large minimum tenancy floor plates. Accommodation of this form of development requires built forms that are large, with high ceiling heights and relatively simple forms. These forms are considerably larger in scale than the heritage buildings.

The key element of scale is the relationship of new and old buildings to the Charles Street and to a lesser extent the Canal Street frontages. Long views to the site should not be dominated by overwhelming new development.

The CMP notes seven concepts:

Concept	Response
1 22 and 24 should be seen from Charles St, from the south and from the rear.	This is achieved. The open frontage allows views along Charles St and the rear of the buildings are seen from the entry arcade to the development.
2 Required infilling of the main street frontages, this anticipated the removal of the corner building.	The corner building is retained and the streetscape is complete.
3 The driveway from Charles St should be infilled with no vehicle access.	This is achieved but part of the gap is used as a pedestrian entry point to the site. This is appropriate given the corner building is being retained.
4 Retain the Canal St wall.	This is achieved and the return wall into the site is also retained above this requirement.
5 Retain four elevations of Canal St warehouse but allow the current annex area to be unfilled for access and additional space.	This is achieved.
6 Retain the corner of Charles and Cimitierre Sts in open form and not infilled with buildings.	This is achieved.
7 Provide an enhanced setting for the Canal St warehouse.	This is not achieved, as Canal Street is the vehicular and service loading area for the site. The building is retained but within its current context of a roadway.

Policies Arising

Policy	Response
1 Design future development contextually in relation to heritage buildings on the site	The new development has been separated from, set back from and arranged in relation to the heritage buildings in Charles Street in particular. The arrangement of the open corner, the use of recovered materials in the entry area and the setting back of new roofs so that they are not visible from street views sets the heritage

Policy	Response
	<p>Buildings as the dominant and principle elements in the street.</p> <p>This is less the case in Canal Street where the new and old are more closely aligned, however setting back new facades behind existing and the retention of space around the smaller buildings assists in establishing appropriate scale changes.</p> <p>Considerable design work has been undertaken in relation to how the buildings, new and old, relate and connect.</p>
2 Design future development contextually in relation to cityscape of Launceston.	The overall massing of the site is consistent with the planning scheme provisions for the site. The new elements of the site are designed as a modern infill building within the setting of Launceston and adopt a particular design approach that relates to the nature of the development.
3 Ensure key vistas to the site and through the site are established.	Key vistas to the site along Charles Street are maintained and conservation of the buildings will enhance those vistas. There are no through site vistas proposed in the development.
4 Create public spaces that take advantage of views and vistas and focus on the heritage buildings	The only public space in the complex of note is the entry court area on Charles and Cimitierre Streets which is a viable public space with activation that provides views to and from the site.
5 Undertake site analysis as part of design.	This was undertaken.

Conservation Works

The CMP policy requires conservation work to be undertaken to the heritage elements of the site. These works were aimed at initially stabilising the place to prevent deterioration and then recovery of the built elements as part of future development.

The project is at a stage where all works are now proposed as one package. It is noted that stabilising works did not take place.

The Proposal does conserve the significant buildings and features, undertakes both repair and conservation works and adapts the buildings, as described in the application, for future use.

This policy will be satisfied by the works following approval.

Policies Arising

Policy	Response
1 Prepare detailed schedules of conservation works for stabilisation	This work was not done but is now proposed as part of this application.

Policy	Response
2 Prepare documentation of conservation works by a qualified person.	The first stage of that documentation is part of the application, further documentation will follow as part of the building approval process. This will be submitted to Heritage Tasmania for final approval. Paul Davies Pty Ltd is engaged to undertake documentation and provide ongoing advice as part of the project.
3 Provide advice to other consultants on heritage issues.	This is being and will be undertaken as part of the overall documentation and construction process.

Archaeological Potential

Considerable work has been undertaken to address the archaeological issues of the site. The earlier assessment and a new Method Statement are submitted to satisfy the archaeological policies.

Policies Arising

Policy	Response
1 Adopt the archaeological potential plan.	Satisfied
2 Retain in situ remains wherever possible	This will be assessed on site, but generally most will be removed as part of the development.
3 Prepare a research strategy	This has been done and is submitted.
4 Develop a process for archaeological monitoring.	This has been done and is submitted.
5 Engage a qualified archaeologist prior to work commencing.	This has been satisfied.

BCA Assessment

Generally the project requires a level of protection, in terms of fire rating and egress provisions, commensurate with the use and number of storeys of development. We are investigating the use of sprinklers in the heritage buildings to address a range of heritage compliance issues.

Policies Arising

Policy	Response
1 Undertake detailed BCA assessment for each heritage building	This has and is being undertaken as part of the design and documentation process.
2 Assess impacts and investigate alternative solutions.	This is being undertaken in response to specific issues.

Interpretation

Interpretation has not been addressed in detail at this stage of the development. It is proposed to undertake interpretation of the place through:

- revealing and using the heritage buildings
- telling stories about the history of the site around the site
- using material revealed through the archaeological process to present the history of the site and its occupants

It is proposed to undertake the archaeology before developing the interpretation program for the site.

Policies Arising

Policy	Response
1 Consider an interpretation plan to guide site interpretation	We propose that this be a condition of consent to be provided to Heritage Tasmania prior to works commencing on site on construction. This will allow archaeological investigation to take place which is likely to impact the plan.
2 consult with community and/or other groups in preparing the plan	See above
3 Develop a staged approach to interpretation. Allow funding to undertake interpretation.	The interpretation will take place in one stage related to construction and to be implemented prior to opening of the centre. Funding will be allocated.
4 Review implementation	This will be done
5 Consider uses and works for their ability to enhance visitor's experience.	The development of bulky goods retailing, while an appropriate use, means the place is not a visitor destination apart from bulk goods shopping. It also means there is likely to be considerable repeat patronage that will limit the effectiveness of some interpretation. The interpretation will have to be direct, be in the most publically accessible locations, attached to the remaining structures and be meaningful to residents visiting the centre.
6 Ensure interpretation is of a high standard and accurate.	This will be done.

Management

This relates to the whole process of preparing applications and managing the future of the site.

Policies Arising

Policy	Response
1 Adopt the CMP	This has been done by the owner, council and the THC.
2 Communicate the CMP to those needing to understand it.	This is being done by incorporation not design and planning, archaeological statements etc.
3 Where policies cannot be satisfied ensure actions do not have adverse heritage impacts.	This has been done and with changes in the condition of the site and the need for re-assessment of elements, decisions have been made to retain heritage value as best can be achieved.
4 Ensure personnel understand legislation and controls relating to heritage.	This has been done.

Conclusion

The proposed development is a balance between retaining and recovering heritage values on the site, complying with the zoning and planning constraints on the site that place quite difficult issues of resolving the built form of new and old development and the viability of undertaking development on the site given the level of constraints and extent of heritage work required.

It is our conclusion that within these constraints the development is appropriate, adequately protects and recovers heritage values and will resolve a long-standing problem that has seen this site deteriorate over many years.

Development of the site is essential to retain the heritage buildings. The key buildings have been on the edge of loss for some time and it is critical that a viable proposal be established that provides for the stabilising and conservation of these structures.

We support the proposal and recommend its approval.